

Scarborough Beach Road West Area

LOCAL DEVELOPMENT PLAN



DOCUMENT HISTORY AND STATUS

**Scarborough Beach Road
West Area
Local Development Plan**

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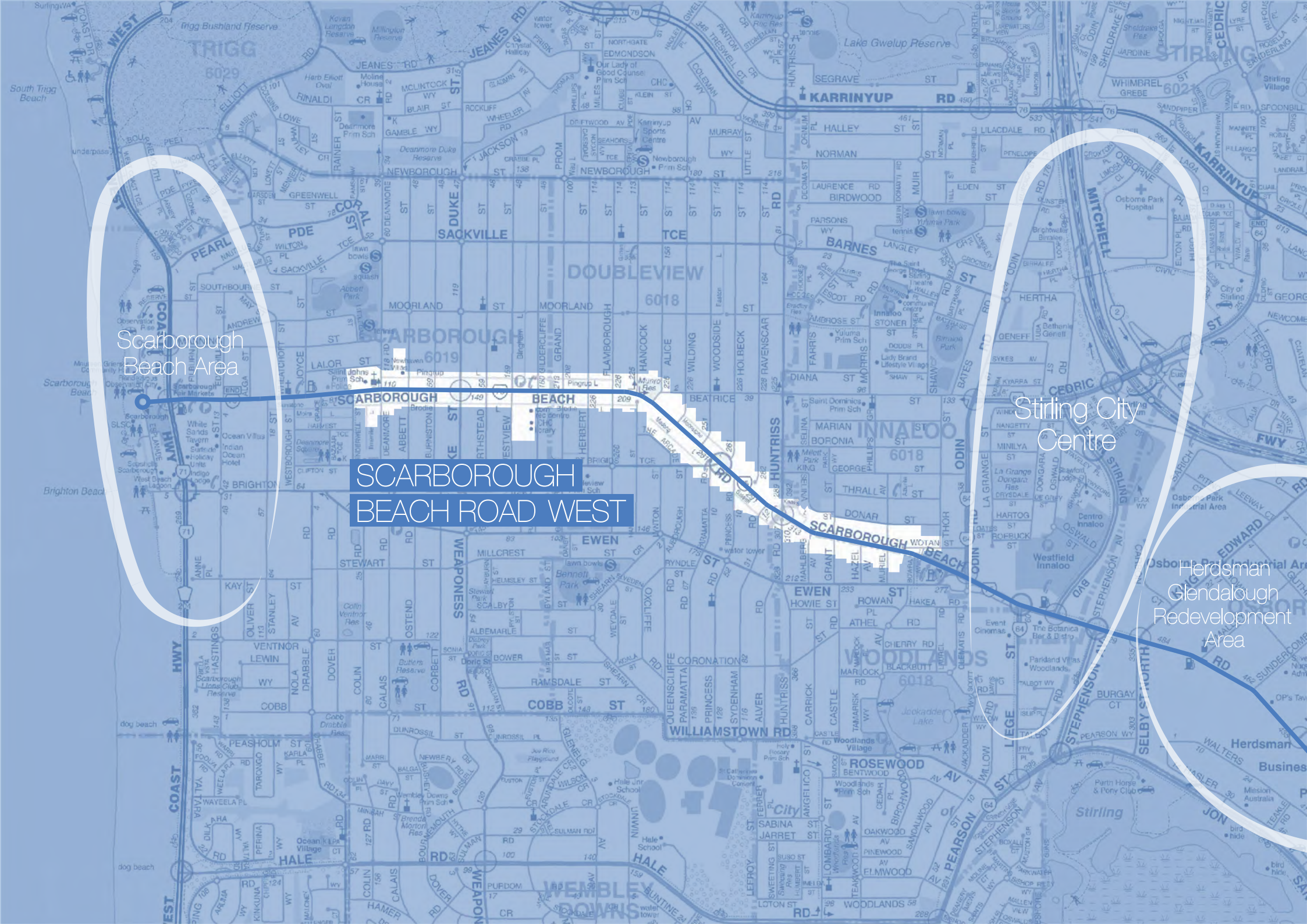
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Scarborough
Beach Area

SCARBOROUGH
BEACH ROAD WEST

Stirling City
Centre

Herdsman
Glendalough
Redevelopment
Area



EXECUTIVE SUMMARY

The Scarborough Beach Road West Local Development Plan has been prepared, to assist in facilitating the growth of the Scarborough Beach Road West area as one of Perth's key Activity Corridors, including two activity centres. The Local Development Plan will be used to regulate land use and development of all private and public land within the Scarborough Beach Road West area Study area shown in **Figure 1: Land Use Plan**.

The vision of the Strategy/Local Development Plan, developed in conjunction with the community, is that:

“The Scarborough Beach Road West Activity Corridor will be a modern, vibrant, mixed use area shared by light rail, pedestrians, cyclists and vehicles, with high quality buildings capitalising on attractive views. The Activity Corridor will accommodate local businesses that serve local residents while taking advantage of passing trade. It will become a street containing many trees that encourages walking and cycling and has places for people to meet.”

The Local Development Plan provides development controls for the Scarborough Beach Road West area which have been developed through community engagement to give effect on the vision.



Existing poor amenity along Scarborough Beach Road



Proposed desirable outcome along Scarborough Beach Road

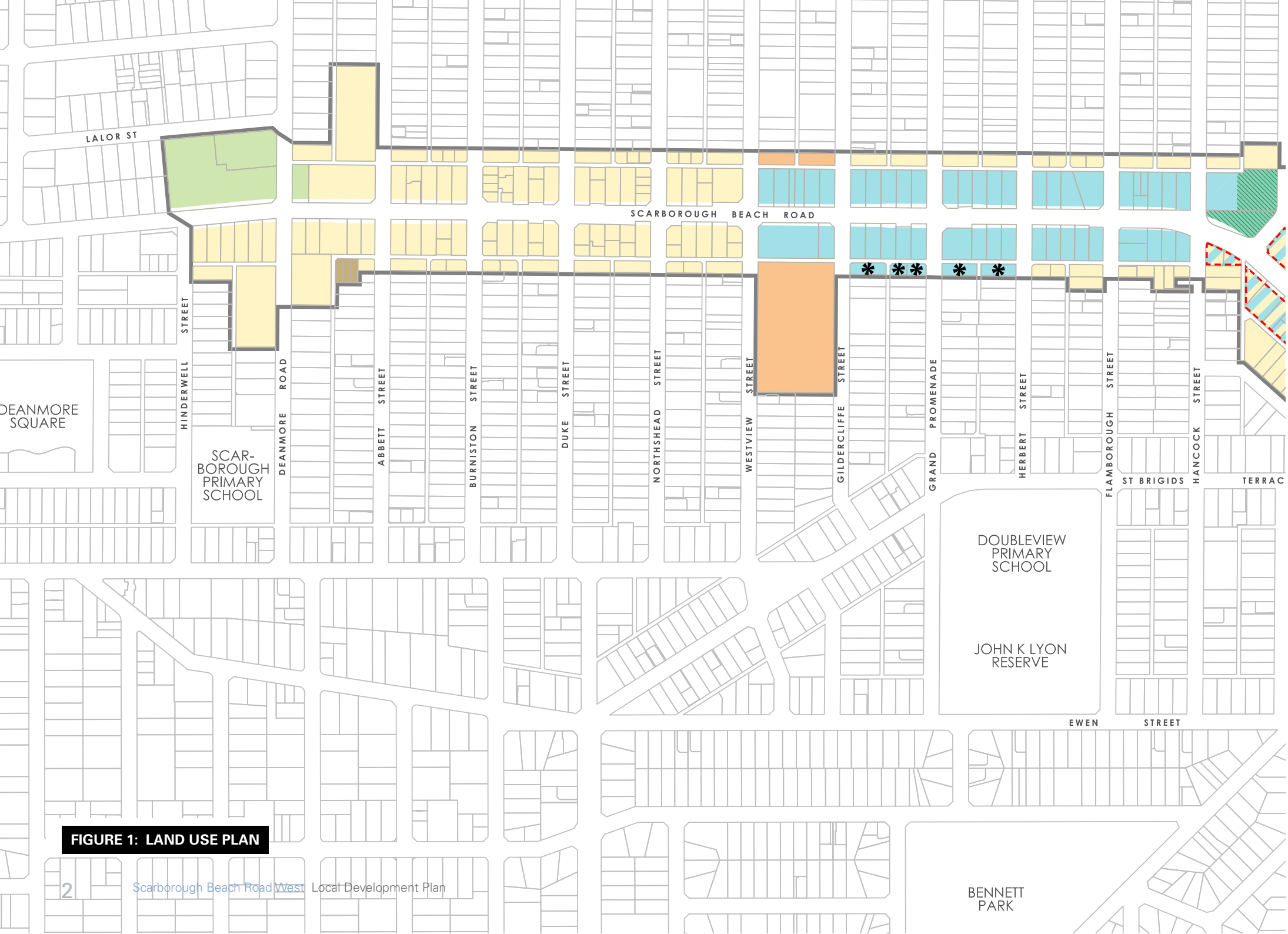
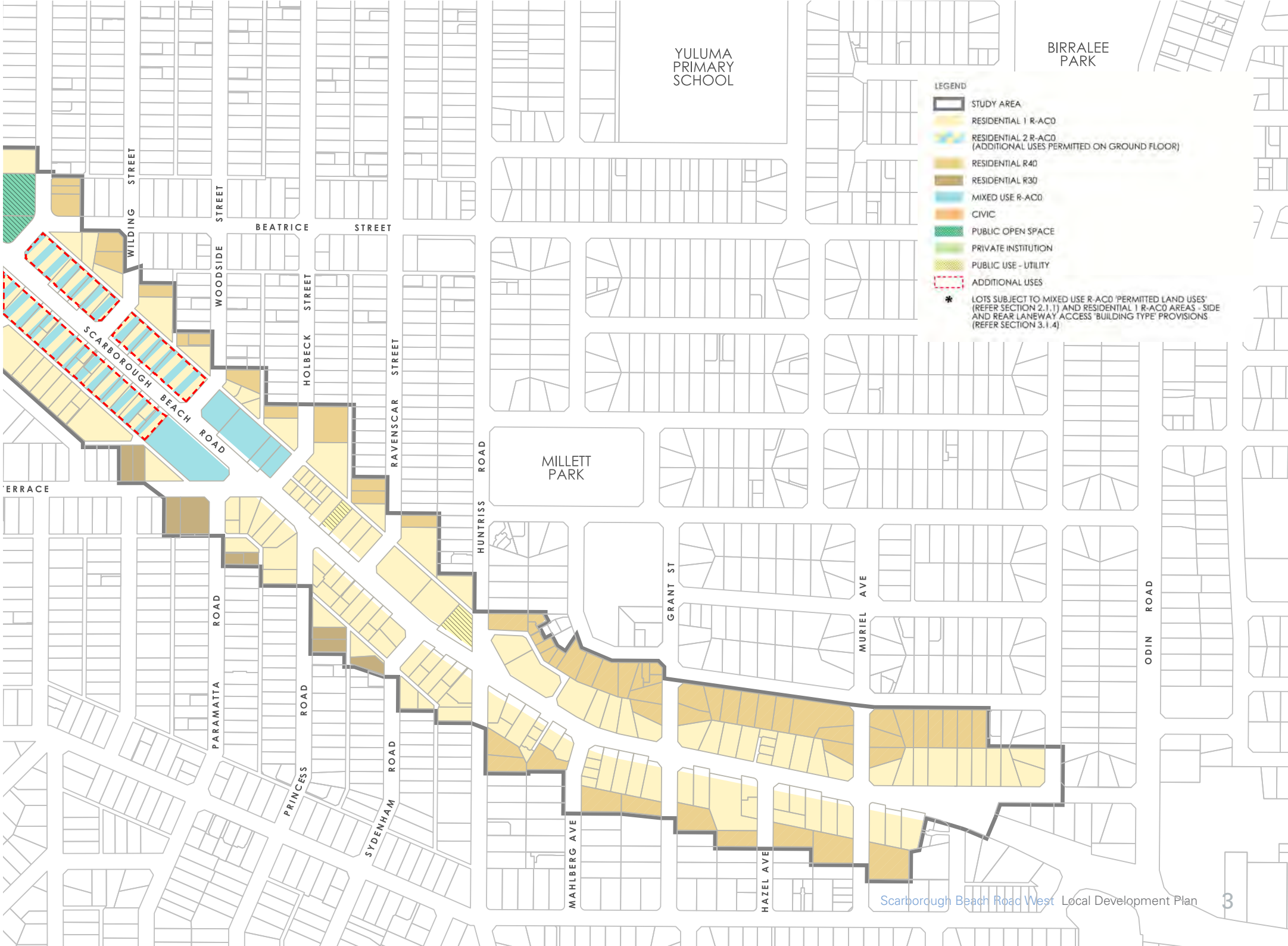


FIGURE 1: LAND USE PLAN



YULUMA
PRIMARY
SCHOOL

BIRRALEE
PARK

LEGEND

- STUDY AREA
- RESIDENTIAL 1 R-AC0
- RESIDENTIAL 2 R-AC0
(ADDITIONAL USES PERMITTED ON GROUND FLOOR)
- RESIDENTIAL R40
- RESIDENTIAL R30
- MIXED USE R-AC0
- CIVIC
- PUBLIC OPEN SPACE
- PRIVATE INSTITUTION
- PUBLIC USE - UTILITY
- ADDITIONAL USES
- *

LOTS SUBJECT TO MIXED USE R-AC0 'PERMITTED LAND USES'
(REFER SECTION 2.1.1) AND RESIDENTIAL 1 R-AC0 AREAS - SIDE
AND REAR LANEWAY ACCESS 'BUILDING TYPE' PROVISIONS
(REFER SECTION 3.1.4)

MILLET
PARK

1. INTRODUCTION

This Local Development Plan has been prepared by the City of Stirling (the City) to fulfil the requirements of Clause 6A.16 of Local Planning Scheme No. 3 .

The Local Development Plan has been prepared to guide development and built form outcomes within the precinct and to ensure the individual development contributes to the realisation of the vision for the area as an efficient and attractive mixed use transit corridor.

The Local Development Plan should be read in conjunction with the Scarborough Beach Road West Activity Corridor Strategy and Landscape Masterplan.

The DAP controls development on Mixed Use R-AC0, Residential 1 R-AC0 and Residential 2 R-AC0 lots, as outlined in the Land Use Plan (refer **Figure 1: Land Use Plan**).

The Local Development Plan is divided into three main sections:

Section 1 – Introduction

A general introduction containing information regarding the purpose and application of the document.

Section 2 – General Provisions

Contains the detailed provisions that are applicable to all development within the precinct.

Section 3 – Specific Provisions

Contains the specific provisions that outline the detailed development requirements for individual lots within the precinct.

1.1

PURPOSE

1. The Local Development Plan will be used by the City in its assessment and determination of development applications.
2. The Local Development Plan is performance based – stating specific design objectives that must be achieved and a related set of performance standards that satisfy the stated objective.

1.2

FORMAT

The provisions within Section 2 - General Provisions are set out in the following framework:

Design Intent

A statement outlining the design philosophy for each Objective.

Objective

Describes the main goal which must be achieved. It is mandatory to meet the objectives.

Acceptable Development Criteria

Performance standards that identify design criteria which will satisfy the specific objective. Compliance with all of the criteria will, through whatever method, achieve the objective. However, individual solutions for complying with the objective may be considered.

1.3

RELATIONSHIP WITH SCHEME & STRATEGY

This Local Development Plan forms part of the City of Stirling’s Local Planning Scheme No. 3, and is to be read in conjunction with the Metropolitan Region Scheme and the Scarborough Beach Road Activity Corridor Strategy.

For guidance on Built Form Vision Philosophy, Principles, Objectives and Framework (including imagery), refer to the Scarborough Beach Road West Strategy; in particular Built Form Framework (section 3.3.3).

The implementation of this Local Development Plan is subject to an Amendment to the Metropolitan Region Scheme and Local Planning Scheme No. 3. Refer to Section 4 Implementation of the Scarborough Beach Road Activity Corridor Strategy.

Where the Scheme, City Policies and/or Local Development Plan do not apply; the provisions of the R-Codes shall prevail.

1.4

HOW TO DETERMINE YOUR DEVELOPMENT POTENTIAL AND REQUIREMENTS IN THE PRECINCT

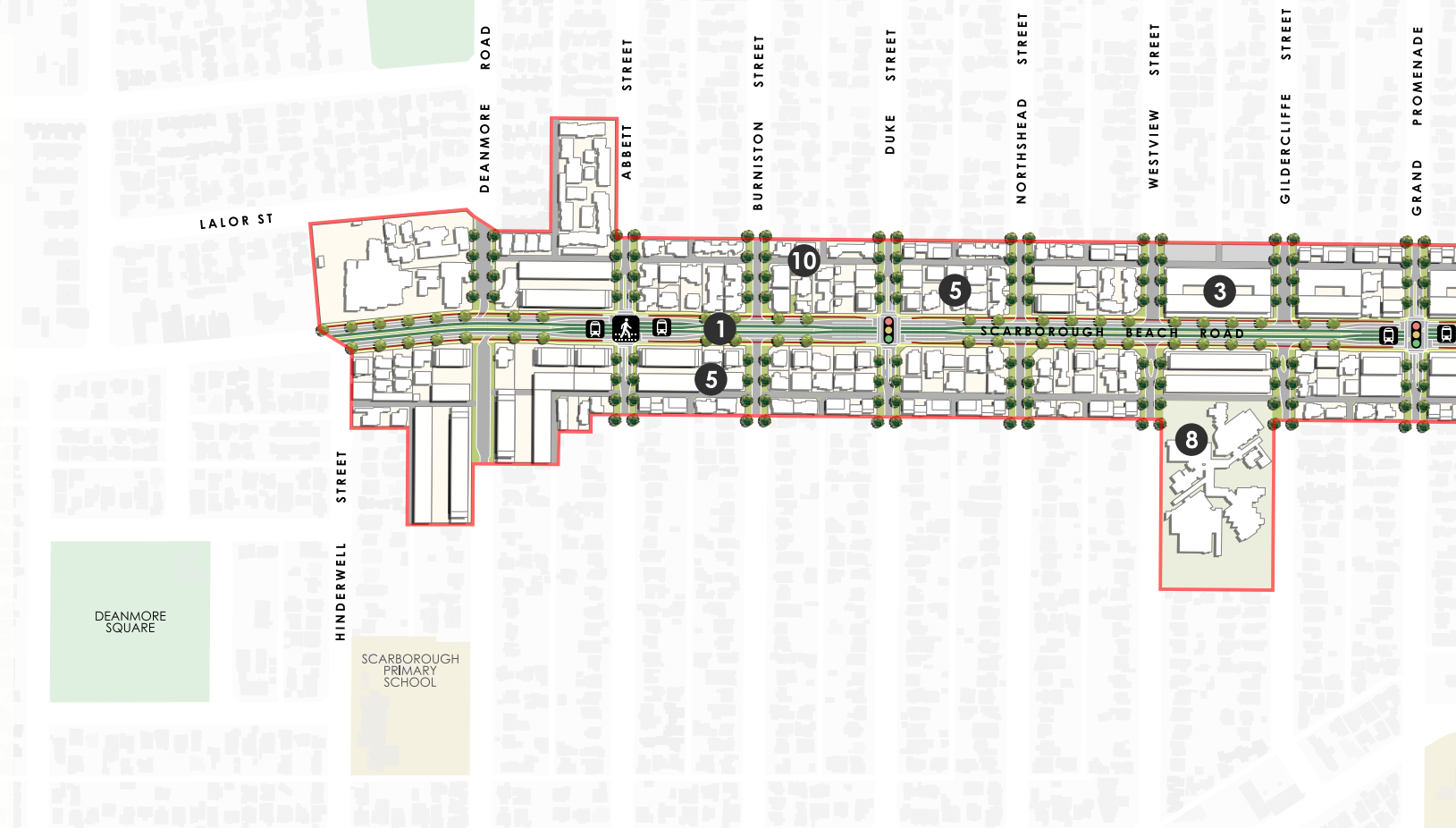
	What?	Where?
Step 1	Determine the zone that applies to your property; and whether the DAP development provisions apply to your zone.	Local Development Plan <ul style="list-style-type: none"> Figure 1: Land Use Plan
Step 2	Identify any special control areas (and associated development requirements) that may apply to your property.	City of Stirling Local Planning Scheme No. 3 <ul style="list-style-type: none"> Part 6: Special Control Areas
Step 3	Determine the specific land uses that are permitted within your property.	City of Stirling Local Planning Scheme No. 3 <ul style="list-style-type: none"> Part 4.3: Table 1 – Zoning Table
Step 4	Determine where these permitted land uses are permitted within your property.	Local Development Plan <ul style="list-style-type: none"> Section 2.1.1: Permitted Land Uses Section 2.1.2: Additional Uses
Step 5	Determine specific built form requirements (i.e. building height, setbacks) that apply to your property	Local Development Plan <ul style="list-style-type: none"> Section 2.2.1 Building Heights Section 3: Specific Guidelines
Step 6	Identify opportunities for built form bonuses.	Local Development Plan <ul style="list-style-type: none"> Section 2.2.2: Plot Ratio and Height Bonuses
Step 7	Determine car parking requirements.	City of Stirling Parking Policy (as amended)
Step 8	Review general development provisions.	Local Development Plan <ul style="list-style-type: none"> Section 2: General Provisions
Step 9	Commence design discussions with City of Stirling.	

LEGEND

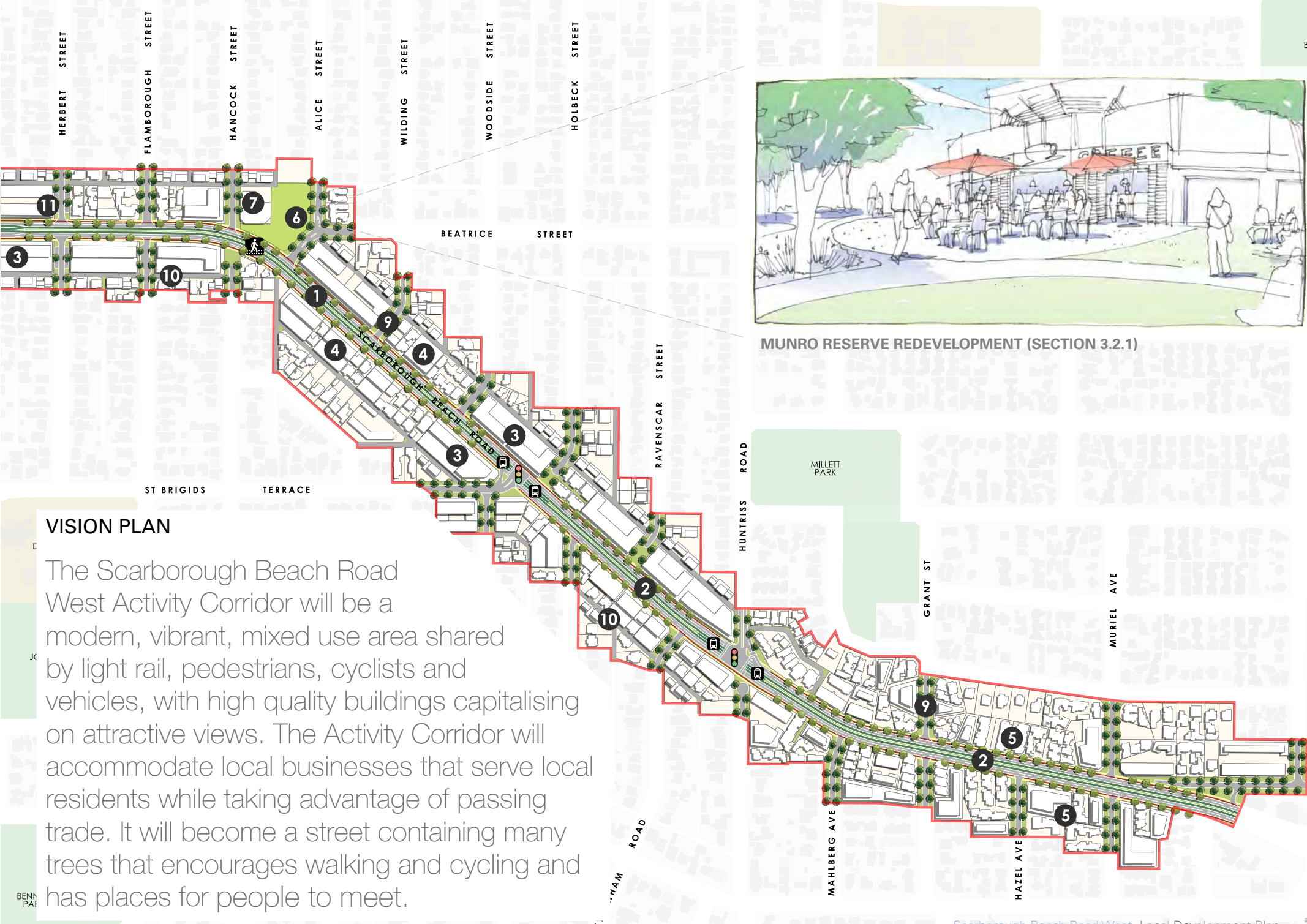
- SCARBOROUGH BEACH ROAD WEST STUDY AREA
- POTENTIAL FUTURE LIGHT RAIL STATION
- EXISTING SIGNALISED INTERSECTIONS (including pedestrian crossing phase)
- POTENTIAL FUTURE PEDESTRIAN ONLY SIGNALISED INTERSECTION/CROSSING POINT

NOTES

- 1** West of St Brigids Terrace - Scarborough Beach Road upgrade accommodating rapid transit (bus/light rail), 2 lanes of traffic, on street parking, pedestrian footpath, Copenhagen cycle path and street trees.
- 2** East of St Brigids Terrace - Scarborough Beach Road upgrade accommodating light rail, 4 lanes of traffic, pedestrian footpath, Copenhagen cycle path and street trees.
- 3** Higher density (4-5 Storey) mixed use development along Scarborough Beach Road with commercial / retail at ground and residential only above.
- 4** Higher density (4-5 Storey) development along Scarborough Beach Road with adaptable residential/commercial at ground and residential only above.
- 5** Higher density (3 Storey) residential development along and adjacent to Scarborough Beach Road.
- 6** Existing Munro Reserve enhanced to create a new active park to socialise, congregate and recreate.
- 7** Redevelopment of existing community building with 5 storey mixed use community building activating Munro Reserve and terminating key vistas along Scarborough Beach Road.
- 8** Celebrate Scarborough Community Centre as a focal point of study area.
- 9** Upgrade side streets to enhance and supplement the amenity along Scarborough Beach Road.
- 10** Upgrade and activate existing laneways through density, pedestrian movement and landscaping opportunities.
- 11** Parking on side streets providing support for mixed land uses on Scarborough Beach Road.



COMMUNITY PLAZA REDEVELOPMENT (SECTION 3.2.2)



MUNRO RESERVE REDEVELOPMENT (SECTION 3.2.1)

VISION PLAN

The Scarborough Beach Road West Activity Corridor will be a modern, vibrant, mixed use area shared by light rail, pedestrians, cyclists and vehicles, with high quality buildings capitalising on attractive views. The Activity Corridor will accommodate local businesses that serve local residents while taking advantage of passing trade. It will become a street containing many trees that encourages walking and cycling and has places for people to meet.

2. GENERAL PROVISIONS

The General Provisions outline the detailed provisions that are applicable to all development within Mixed Use R-AC0, Residential 1 R-AC0 and Residential 2 R-AC0.

2.1.1 PERMITTED LAND USES

Permitted land uses within the Mixed Use R-AC0, Residential 1 R-AC0 and Residential 2 R-AC0 areas of the Scarborough Beach Road West Study Area, as indicated on the Land Use Plan (refer **Figure 1: Land Use Plan**), are to be in accordance with their respective zones as specified within the City of Stirling Local Planning Scheme No. 3.

The permitted uses are allocated as follows:

MIXED USE R-AC0

Ground floor: All permitted uses (no residential fronting Scarborough Beach Road)

Upper floors: Residential only

RESIDENTIAL 1 R-AC0

Ground/Upper floors: Residential only

RESIDENTIAL 2 R-AC0

Ground floor: Residential plus additional uses specified within section 3.1.3.

Upper floors: Residential only

Note: Active frontages for ground floor uses within Mixed Use/Residential 2 areas, is further defined under Section 2.2.5.

The following development controls apply on all Mixed Use R-AC0, Residential 1 R-AC0 and Residential 2 R-AC0 Lots, unless specifically referred to below:

- New multiple dwelling development is permitted.
- New single house development is not permitted.
- Group Dwelling Development is permitted only on Residential 1 R-AC0 lots.
- Proposed development over existing strata lots shall only be permitted over the comprehensive parent strata lot.
- Built strata subdivision is permitted.
- Single lot subdivision is not permitted.
- Survey strata subdivision is only permitted on Residential 1 R-AC0 lots.

2.1.2 ADDITIONAL USES

The use class permissibility outlined within the City of Stirling Local Planning Scheme No. 3 is supplemented with the following Additional Use Classifications for the Residential 2 R-AC0 zone, as outlined within **Figure 1: Land Use Plan**:

- Office
- Consulting Rooms
- Personal Care Services

Note: All existing land uses rendered non-conforming by this document can continue until redevelopment occurs, at which time compliance with section 2.1.1 and 2.1.2 shall apply.

2.2.1 BUILDING HEIGHTS

DESIGN INTENT

In conjunction with defined setbacks and all other applicable development provisions, building heights have been defined to control the physical and visual amenity of development; together with achieving an appropriate streetscape within the precinct.

Building heights within the precinct, as indicated on the **Figure 3: Building Heights Plan**, are controlled based on land use precincts. That is, minimum two/maximum five storey buildings are permitted within 'Mixed Use R-AC0' and 'Residential 2 R-AC0' areas and minimum two/maximum three storey buildings permitted within 'Residential 1 R-AC0' areas.

OBJECTIVE

To control building heights to ensure the desired character and proportions of the streets, whilst providing the opportunity for new landmark buildings.

ACCEPTABLE DEVELOPMENT CRITERIA

Building heights shall comply with the following:

Section 3.1.1 - 3.1.4 - Building Height Table, as applicable.

Section 2.2.2 - Plot Ratio and Height Bonuses

Section 2.2.4 - Mandatory Finished Ground Floor Levels

Ground floor level of development on Mixed Use R-AC0 and Residential 2 R-AC0 lot shall be consistent with the adjacent verge level, unless where not practical.

Ground floor level of development on Residential 1 R-AC0 lots shall be a maximum of 1.2m above the adjacent footpath level.

2.2.2 PLOT RATIO AND HEIGHT BONUSES

DESIGN INTENT

To achieve the high quality public realm outcomes along Scarborough Beach Road, a road widening is required. As specified in **section 2.7.2** in accordance with **Planning Control Area 123**, road widenings along Scarborough Beach Road vary between 0-11 metres.

An additional development bonus shall be provided, in the form of plot ratio and height bonuses, to land owners who are required to cede land for the widening of Scarborough Beach Road.

OBJECTIVE

To achieve an equitable outcome, the City will provide an additional development bonus, in the form of plot ratio and height bonuses, to land owners who are required to cede land for the widening of Scarborough Beach Road.

ACCEPTABLE DEVELOPMENT CRITERIA

The development bonuses below are based on the extent of land (width in metres) ceded for Scarborough Beach Road widening in accordance with **Planning Control Area 123**.

The below plot ratio/height bonuses are on top of the permitted plot ratios/height specified within **sections 3.1.1–3.1.4**.

Scarborough Beach Road Widening (1)	Plot Ratio Bonus	Building Height Bonus
0m - 2.99m	0.25	0
3m - 5.99m	0.5	0
6m - 8.99m	0.75	1 storey (2)
9m +	1.0	1 storey (2)

(1) Based on average widening.

(2) Does not apply to Mixed Use R-AC0 and Residential 2 R-AC0.

Note: Land ceded for road widening purposes can be included in plot ratio calculations.

Figure 2 opposite illustrates height controls and mandatory ground floor levels (refer section 2.2.4) within Residential 1 R-AC0 areas - no laneway access (refer section 3.1.3).



LEGEND

- STUDY AREA
- BUILDING HEIGHT - MIN 2 STOREYS: MAX 3 STOREYS
- BUILDING HEIGHT - MIN 3 STOREYS: MAX 5 STOREYS
- BUILDING HEIGHT - AS PER RESIDENTIAL DESIGN CODES
- PUBLIC OPEN SPACE

Note: Building Heights subject to an additional 1 storey height bonus, subject to **section 2.2.2**

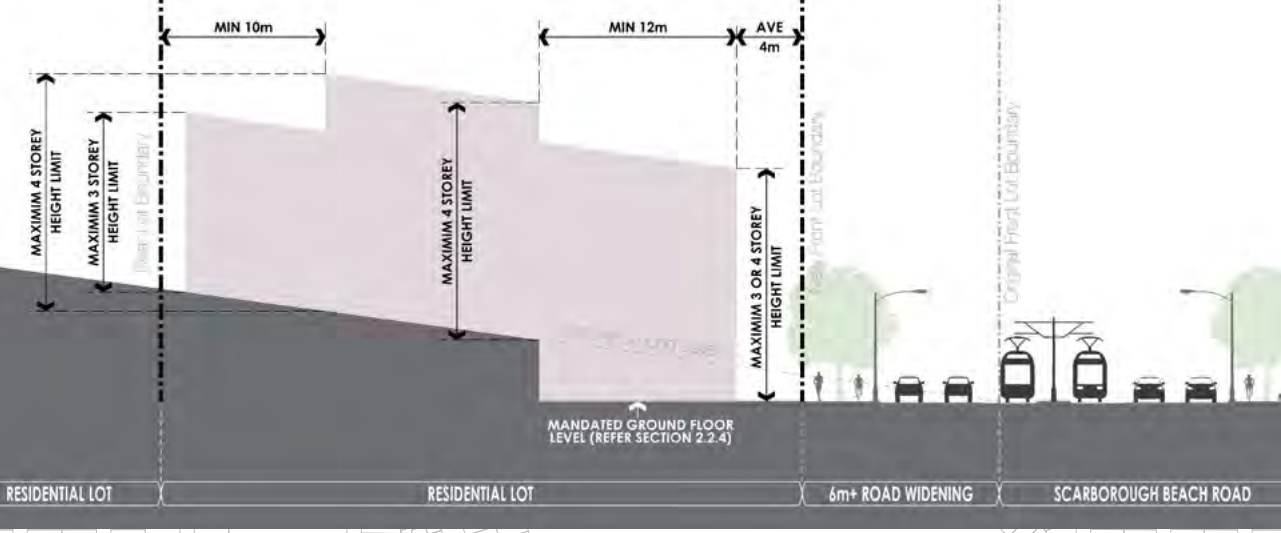


FIGURE 2: HEIGHT CONTROLS AND MANDATORY GROUND FLOOR LEVELS WITHIN RESIDENTIAL 1 R-AC0 AREAS - NO LANEWAY ACCESS

FIGURE 3: BUILDING HEIGHTS PLAN

2.2.3 CHARACTER FACADES

DESIGN INTENT

Character facades (including awnings) along Scarborough Beach Road shall be retained, incorporated and interpreted within their respective redevelopment sites in order to assist with creating a sense of place and to respect the specific character of the precinct; as illustrated in **Figure 4: Character Facade Locations to be Protected**.

OBJECTIVE

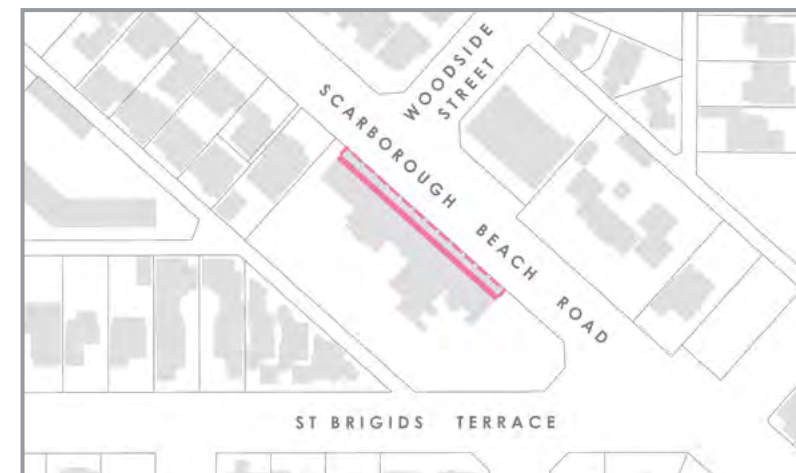
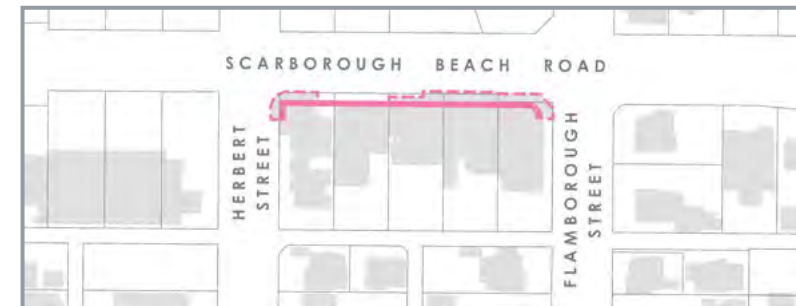
To enrich the urban fabric within the Scarborough Beach Road West area with elements of character.

ACCEPTABLE DEVELOPMENT CRITERIA

Character Facades (including awnings) shall be retained in the following locations, as illustrated opposite:

- Along the Northern side of Scarborough Beach Road, between Westview Street and Gildercliffe Street;
- Along the Southern side of Scarborough Beach Road between, Herbert Street and Flamborough Street; and
- Lot 20 along the Southern side of Scarborough Beach Road, between St Brigids Terrace and Woodside Street.

Note: Refer to the Building Setback Table under Section 3.1.1, for development provisions to protect and pronounce these character facades.



LEGEND



-  CHARACTER FACADE TO BE PROTECTED
-  CHARACTER AWNING TO BE PROTECTED

FIGURE 4: CHARATER FASCADE LOCATIONS TO BE PROTECTED

2.2.4 MANDATORY FINISHED GROUND FLOOR LEVELS

In order to facilitate ease of access into Residential 1 R-AC0 lots without laneway access between Huntriss Road and Thor Street, the ground floor levels (ahd) adjacent to Scarborough Beach Road shall be as per **Figure 5: Finished Ground Floor Levels Adjacent to Scarborough Beach Road.**

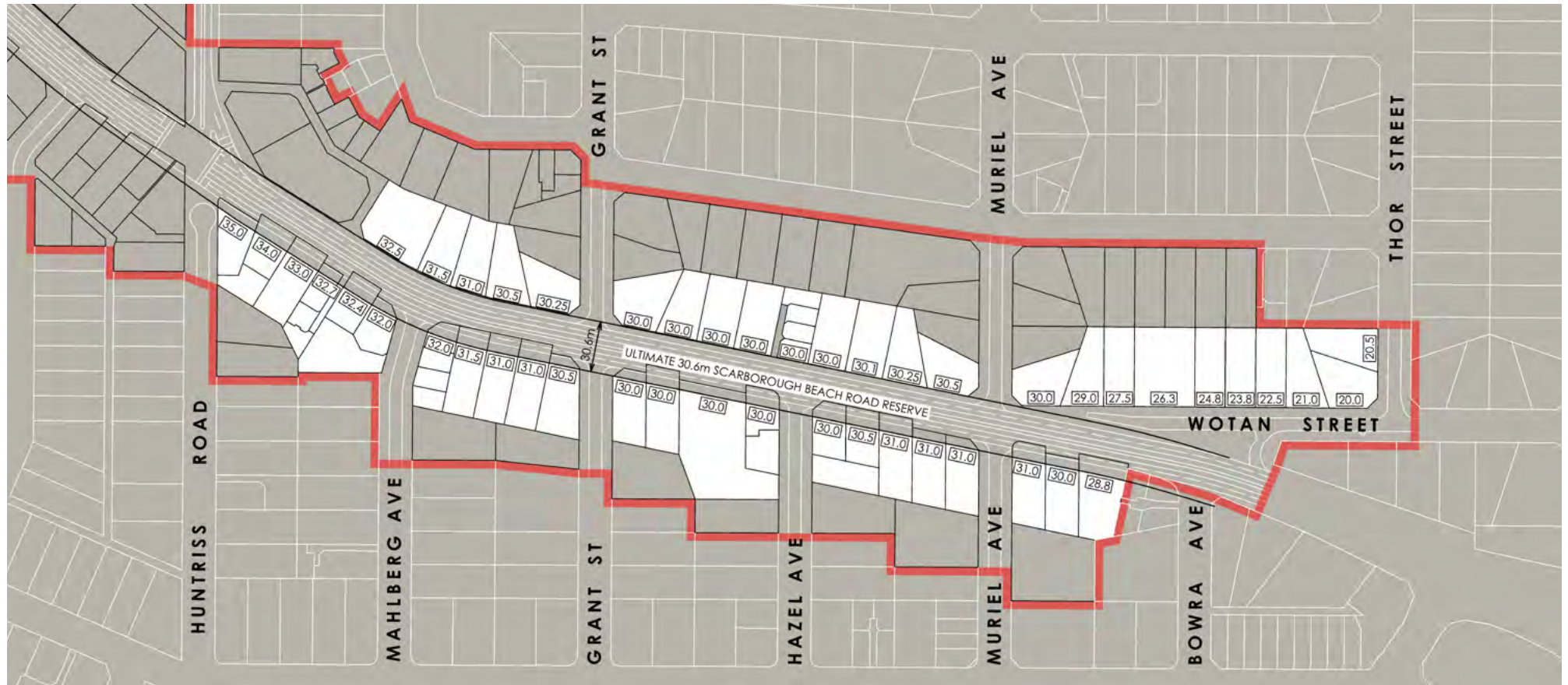


FIGURE 5: FINISHED GROUND FLOOR LEVELS ADJACENT TO SCARBOROUGH BEACH ROAD

2.2.5 ACTIVE FRONTAGE TO MIXED USE R-AC0 / RESIDENTIAL 2 R-AC0

DESIGN INTENT

Design and articulation at street level helps to provide for a vibrant and stimulating pedestrian experience. At street level, the building frontage of commercial and retail tenancies should be designed to address the street via entries, windows and displays. The structural grid of the building should be designed to allow for a range of retail and commercial floor plate sizes. Fine-grained, street-based tenancies are preferred within the mixed use and business zones addressing Scarborough Beach Road.

Vendor activity may extend beyond the site boundary to encourage alfresco seating and the display of goods. Where vendor activity is proposed to extend beyond the site boundary, the applicant or alfresco vendor is to consult with the City regarding the leasing and management of the public space.

OBJECTIVES

- To create a vibrant and stimulating pedestrian experience.
- To create opportunity for passive surveillance of the public realm contributing to the sense of safety.
- To encourage retail tenancies containing active frontages and address Scarborough Beach Road.

ACCEPTABLE DEVELOPMENT CRITERIA

On Mixed Use R-AC0 and Residential 2 R-AC0 lots, ground floor built form shall provide an active frontage (includes residential and all non-residential uses) to Scarborough Beach Road, returning a minimum 12m along the side street (as applicable), as illustrated in **Figure 6: Ground floor active frontage and built form distribution plan (within Mixed Use R-AC0 and Residential 2 R-AC0 Developments)** opposite and specified below:

- Incorporate glazing to a minimum of 75% of the street frontage, with a minimum head height of 3.0m and sills at a height no greater than 500 mm.
- Incorporate transparent, non-reflective glazing only. Exceptions may be considered in respect of situations where privacy issues require frosted or opaque glazing or to screen service areas or structural elements.
- Incorporate architectural treatment and articulation to create interest on the street and to break up facades.

On all lots:

- Ground floor built form shall address Scarborough Beach Road and shall address the side street.

Where ground floor built form to the side street is unachievable due to accommodating parking provisions (on lots under 25m wide only), appropriate screening to parking areas shall be provided to create visual interest from the side street.

- Development adjacent to or above side streets/laneways shall have windows to non-residential floorspace and habitable spaces to encourage passive surveillance and engagement with the side street/laneway.
- Fencing to side streets/laneways shall be visually permeable above 1.2m high.



FIGURE 6: GROUND FLOOR ACTIVE FRONTAGE AND BUILT FORM DISTRIBUTION PLAN (WITHIN MIXED USE R-AC0 AND RESIDENTIAL 2 R-AC0 DEVELOPMENTS)

Built form examples with active frontages to ground floor

2.2.6 ARCHITECTURAL EXPRESSION AND ARTICULATION

DESIGN INTENT

High quality buildings can have a lasting contribution to the public realm, providing pedestrian friendly streetscapes and contributing to the built fabric of the precinct. The precinct should achieve a high standard of architectural design that offers a contemporary aesthetic through a variety of distinctive forms.

The use of textures, materials and colour can articulate the facade, internal layout and reflect the structure rather than applied as decoration.

OBJECTIVE

To generate interesting, innovative and creative architectural buildings. Exhibit a 'fine grain' character at the street level to ensure a human scale street edge. Reduce building bulk and massing and respond to adjacent buildings.

ACCEPTABLE DEVELOPMENT CRITERIA

- All development shall provide variation in building articulation, texture and colour to reduce overall bulk and create interest, and incorporate design elements such as balconies and windows.
- Built form abutting laneways should be articulated to create visual interest. Blank walls shall not be permitted facing the street or laneways.
- Where building frontages are more than 20m in width, building massing should be articulated or "broken up" to ensure that facades are not overly long. This can be achieved by either:
 - Creating the visual impression of multiple buildings on wide sites viewed from the public realm; or
 - Breaking up the facades through the use of vertical breaks above ground floor.



Examples of built form articulation and architectural expression

2.2.7 CORNER SITES

DESIGN INTENT

Corner buildings provide a transition between streets and define the public realm at intersections. The design of a building at the corners shall be considered carefully to ensure there is continuity or harmony of materials and detailing to both elevations. The corner may be emphasised by height, form or feature elements such as wrapping balconies.

OBJECTIVES

- To ensure that built form on corner sites reinforces the street edge.
- To ensure building design considers the site truncation by way of design and detail.
- To ensure appropriate design of corners to ameliorate wind conditions at ground level.

ACCEPTABLE DEVELOPMENT CRITERIA

- Walls on corner frontages shall be articulated and provide major openings overlooking the public domain along Scarborough Beach Road. Blank walls to corner frontages shall not be supported.
- Vertical emphasis on corners may be considered, up to a maximum of 1 storey above building height limit as specified in sections 3.1.1 - 3.1.4 (inclusive), as applicable (including 1 storey above bonus heights – refer section 2.2.2).



Corner buildings reinforcing street edge and building mass at corners

2.2.8 BUILDING ADAPTABILITY

DESIGN INTENT

The intention in the Residential 2 R-AC0 zone within the precinct is to encourage ground floor commercial uses adjacent to Scarborough Beach Road, with the building adaptability to change its use to other active commercial uses over time, when it is potentially re-zoned to Mixed Use.

For example, designing a building with movable walls/partitions allows for different users to change the space. Additionally, using sustainable design allows for a building to adapt to different environments and conditions. Adaptable design also facilitates retrofitting existing buildings rather than building anew.

OBJECTIVES

Ensure where possible and practical, the design of ground floor built form within the Residential 2 R-AC0 zone and adjacent to Scarborough Beach Road, has the building adaptability to be ultimately convertible to other active commercial uses over time (subject to future planning).

ACCEPTABLE DEVELOPMENT CRITERIA

Ground floor built form within the Residential 2 R-AC0 zone, and adjacent to Scarborough Beach Road, shall:

- Demonstrates the provision of Universal Access.
- Provide for future modification, extension and adaptation for additional or alternate uses.
- Provide flexible building layouts which allow greater adaptability of the floor area of, or tenancies on, the first floor of a building above the ground floor.
- Have a minimum floor to ceiling height of 3.4m in order to provide for flexibility of future use.
- Location of load bearing walls and columns in a regular grid pattern, allowing for the addition or removal of dividing walls for the reconfiguration of floor plans within the building envelope.



Buildings providing adaptable ground floor to allow for change of use over time

2.2.9 ROOF FORMS

DESIGN INTENT

The design of the roof should be considered as part of the overall building composition. Service elements should be integrated into the roof design.

The roofline can be used to reduce large building masses and assist with articulating the building facade. The roof top can provide an area for passive recreation.

OBJECTIVE

- Roof forms (including roof gardens) are to be an integral aspect of the overall building design and assist in reducing the impact of building mass and service equipment.

ACCEPTABLE DEVELOPMENT CRITERIA

- New buildings shall be designed to contribute positively to the skyline with their design as an integral feature of the built form, and shall be in accordance with the Specific Guidelines (refer sections 3.1.1 - 3.1.4 inclusive, as applicable).
- Roof design shall incorporate plant and lift overruns as an integral component of the built form design.
- Roof gardens incorporated into roof design are strongly encouraged wherever practical.



Examples of roof forms that integrate with the overall building design and assist in reducing building mass

2.2.10 COLOURS & MATERIALS

DESIGN INTENT

Material and colour selection shall respond to the coastal location and local context. This includes the use of natural materials and light and neutral colours with accents, highlights and feature colours derived from the local natural palette. When selecting building materials, claddings and finishes, material longevity and maintenance should be considered.

OBJECTIVES

- To ensure long life and ease of maintenance for the development.
- To use materials and colours to articulate the building façade and overall design, and respond to the coastal location and local context.

ACCEPTABLE DEVELOPMENT CRITERIA

- Each building shall comprise more than one facade material (excluding glazing), with material variation corresponding to surface changes, internal layout and feature elements.
- Built form design shall incorporate materials and finishes that are robust and easy to maintain in the context of the prevailing climate.
- Bright and bold colours should be used as a minor accent or limited expression and not as the dominant style.
- Green walls and vertical landscaping of building facades are strongly encouraged and shall be incorporated wherever practical.



Accent and highlight colours can be derived from natural environment also.

Example colours that have been derived from the local natural palette. Colours depicted are not an exclusive list but an example of how to derive colour from the local natural palette.

2.2.11 NOISE ATTENUATION

DESIGN INTENT

The development of new buildings in close proximity to Scarborough Beach Road needs to consider the potential for noise intrusion. This issue must be addressed at the planning and design stage to ensure that appropriate measures are taken to minimise impacts.

OBJECTIVE

Ensure all developments are designed and constructed to incorporate acoustic attenuation measures and materials as far as is practical and viable.

ACCEPTABLE DEVELOPMENT CRITERIA

Developments adjacent to Scarborough Beach Road are to include noise attenuation measures such as:

- An acoustic report (including a noise management plan) for any noise generating use shall be required to be submitted with development plans at the Development Application stage to demonstrate noise will comply with accepted/relevant standards (i.e. Australian Standards, Building Code of Australia and/or National Construction Codes).

2.2.12 AWNINGS

DESIGN INTENT

Development within the mixed uses areas shall provide pedestrian cover. Awnings and canopies provide protection from sun, rain and wind, encourage pedestrian activity and create opportunities for extending retail activities to footpaths such as dining and vendor activities. They also create an intimacy of space. The design of the awning and canopy can provide identity and detail to a building. Awnings and canopies can be used to emphasise corners and define entry foyers to upper levels via accentuated height or a variation in design. High level awnings such as shading over windows are encouraged to add interest and expression to the building's architecture and improve its energy efficiency.

OBJECTIVE

- To provide continuous shelter along the street edge. To encourage pedestrian activity as part of an activated built form environment.

ACCEPTABLE DEVELOPMENT CRITERIA

- On Mixed Use R-AC0 lots, awnings shall be provided along Scarborough Beach Road for continuous shelter.
- Awning height - Minimum 2.7m; Maximum 3.2m
- Awning width - Minimum 2.0m.

2.2.13 OVERLOOKING AND PRIVACY

DESIGN INTENT

In any urban environment it is likely that some overlooking will occur, however residential dwellings should be designed to deliver a reasonable level of privacy. Internal layouts should consider the activities of each of the areas where overlooking may occur, the times and frequency these spaces are being used and the occupants expectations of privacy.

Overlooking from residential dwellings is to be directed towards the streets, laneways, private/communal open space (belonging to the individual development) and public open space.

The elevation and facade composition should not be compromised in achieving privacy between residential dwellings, which should be achieved via well considered building configuration.

OBJECTIVES

- To achieve a balance between visual privacy between apartments without compromising outlook, views and natural lighting to dwellings.
- To provide appropriate building separation between and within sites to allow for adequate visual privacy to internal spaces.

ACCEPTABLE DEVELOPMENT CRITERIA

- Incorporate windows that overlook streets, communal open space, rights of way and other public spaces, especially at lower levels or podiums to provide passive surveillance to the public realm.
- Balconies are to overlook streets, communal open space, rights of way and other public spaces.
- Locate and design habitable rooms, major openings and private open spaces (including balconies) to minimise the potential for overlooking of adjacent private spaces, screening devices are not permitted.
- Visual Privacy, in relation to section 6.4.1 of the R-Codes, does not apply.
- Highlight windows to habitable rooms are not permitted if they are only window/windows to the habitable room.
- Optimise views of the city and/or ocean at upper levels through the location of windows and amenities.



Example of laneway overlooked by adjacent development.

2.3 ACCESS AND PARKING

2.3.1 VEHICLE ACCESS

DESIGN INTENT

To ensure provision of safe, secure, accessible and visually acceptable parking for residents and workers, it is important to minimise the visual impact of access to parking areas and garages by incorporating these elements as integral components of the site and building design. Vehicle movement must not compromise pedestrian movement and safety.

Where a laneway exists or is proposed, vehicle access shall be provided via this laneway or from a side street.

OBJECTIVE

Ensure that the design and location of vehicle access and circulation does not compromise pedestrian movement and safety or the design quality of the building and public spaces. The needs of all pedestrians including children, disabled and aged should be considered in the design of roads, intersections and accessways.

ACCEPTABLE DEVELOPMENT CRITERIA

- Vehicle access to car parking areas shall be provided via laneways wherever possible; however, access from the side street is acceptable within 15m of the laneway (refer **Figure 7: Permitted Vehicle Access**).
- Vehicle access off Scarborough Beach Road is not permitted for lots with laneway or side street access.
- All vehicles should be able to enter and leave sites in a forward gear.
- Vehicle access shall be designed and detailed as an integral component of the development and be incorporated into the design treatment of the streetscapes.
- Loading docks, overhead doors and other service entries are prohibited on non-laneway facing facades (refer **Figure 7: Permitted Vehicle Access**).
- Separate entries shall be provided for vehicular and cycle access.
- Wherever possible, buildings shall be constructed up to and above a vehicle accessway on the lot.

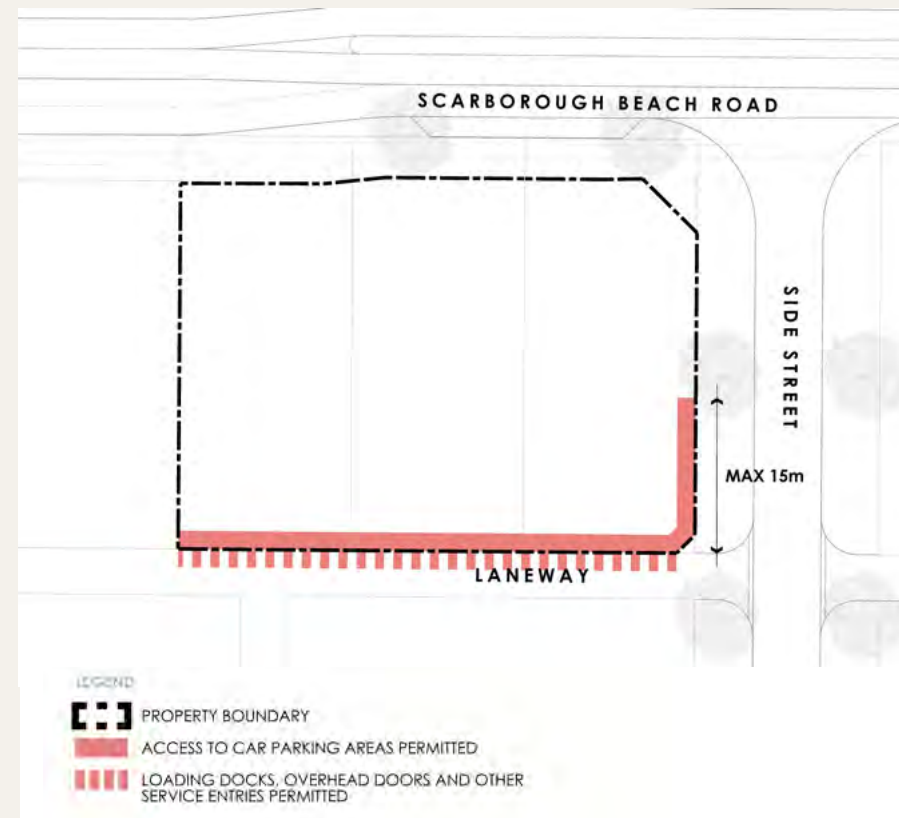


FIGURE 7: PERMITTED VEHICLE ACCESS.

2.3.2 CROSSOVERS

DESIGN INTENT

Crossovers to Scarborough Beach Road will only be allowed to lots without other adequate forms of access from side streets or laneways. Lots are encouraged to amalgamate to achieve access from side streets, however should this be impractical, the following shall apply.

Development will be designed to minimise the extent of crossovers onto Scarborough Beach Road such that the streetscape is primarily used for pedestrian activity, on-street car parking and landscaping.

OBJECTIVE

Developments will provide dedicated access to development areas from the public street, but will minimise the prevalence of crossovers within the streetscape.

ACCEPTABLE DEVELOPMENT CRITERIA

- Vehicle crossovers shall be located and designed to minimise impact on pedestrian and cyclist movement.
- A maximum of one crossover per lot shall be permitted.
- Crossovers shall be located to avoid key vehicle parking, landscape or pedestrian walkways.

2.3.3 PEDESTRIAN ACCESS

DESIGN INTENT

Development will be designed to cater for pedestrian access as a priority to ensure that each development makes a contribution to the walkability of the locality and the ease of access to development within the locality.

OBJECTIVE

Developments will provide safe and secure universal access for pedestrians to main entrances of development and vehicle parking areas.

ACCEPTABLE DEVELOPMENT CRITERIA

- Development Applications shall identify and justify pedestrian access to and within a proposed development.
- Development shall provide a dedicated pedestrian access to the front entrance, including access for persons with disabilities.
- Pedestrian access to vehicle parking areas shall be separated from vehicle movement areas.
- Careful consideration must be given to the placement of signposts, utility poles, bins, street furniture etc. to ensure a suitable clear path is available for use by pedestrians and cyclists.

2.3.4 ABOVE GROUND/SLEEVED PARKING

DESIGN INTENT

Parking structures should not be visible from the public domain and be integrated as part of the built form.

OBJECTIVE

To ensure that multistorey carparks are not visible from the street and are sleeved by active or residential uses.

ACCEPTABLE DEVELOPMENT CRITERIA

Car parking bays located within a building shall be located in accordance with

Figures 8 - 12.

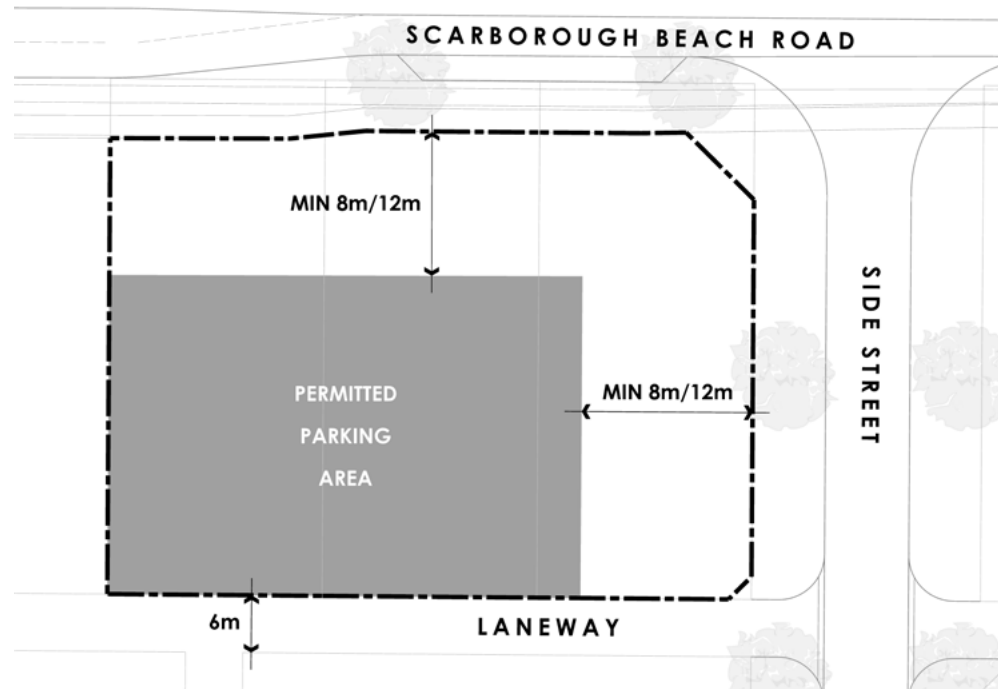


FIGURE 8: PERMITTED PARKING AREAS TO GROUND AND UPPER FLOORS

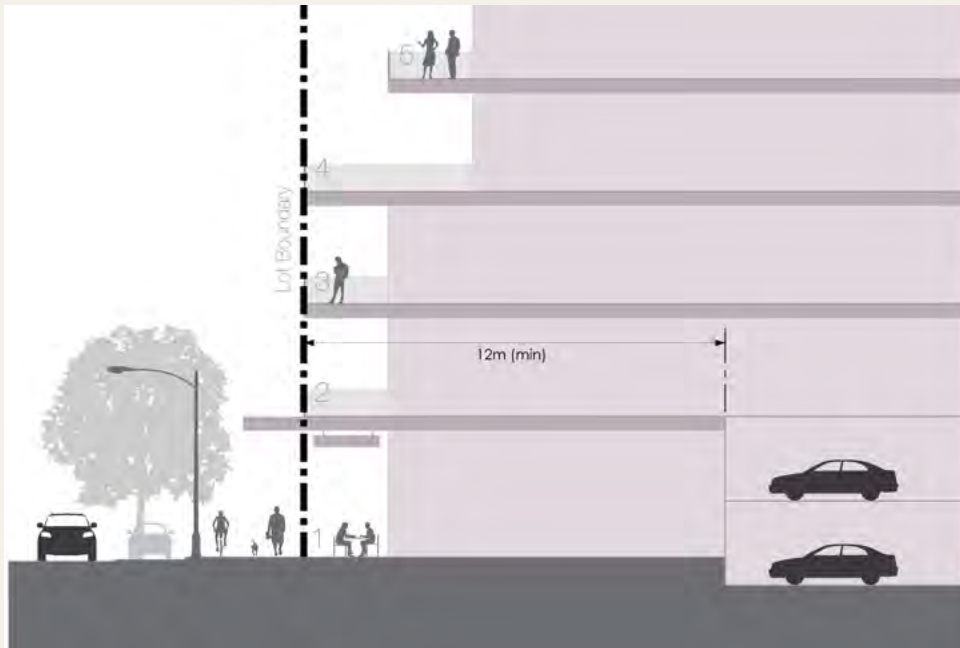


FIGURE 9: MIXED USE R-AC0 AND RESIDENTIAL 2 R-AC0 PARKING - MULTI-STOREY CAR PARKS SLEEVED BEHIND BUILT FORM

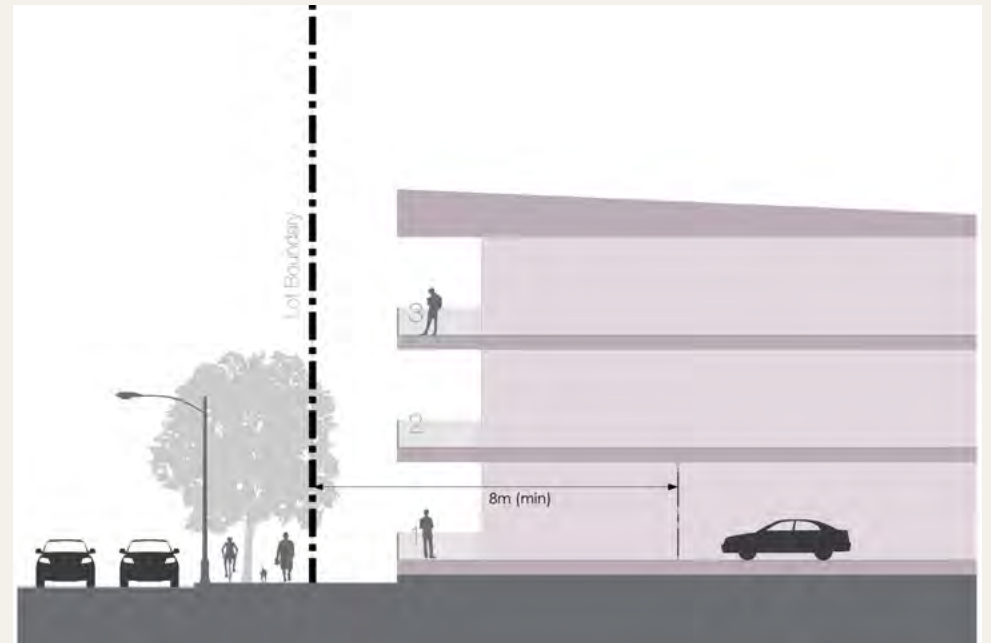


FIGURE 11: RESIDENTIAL 1 R-AC0 PARKING - MULTI-STOREY CAR PARKS SLEEVED BEHIND GROUND FLOOR USES

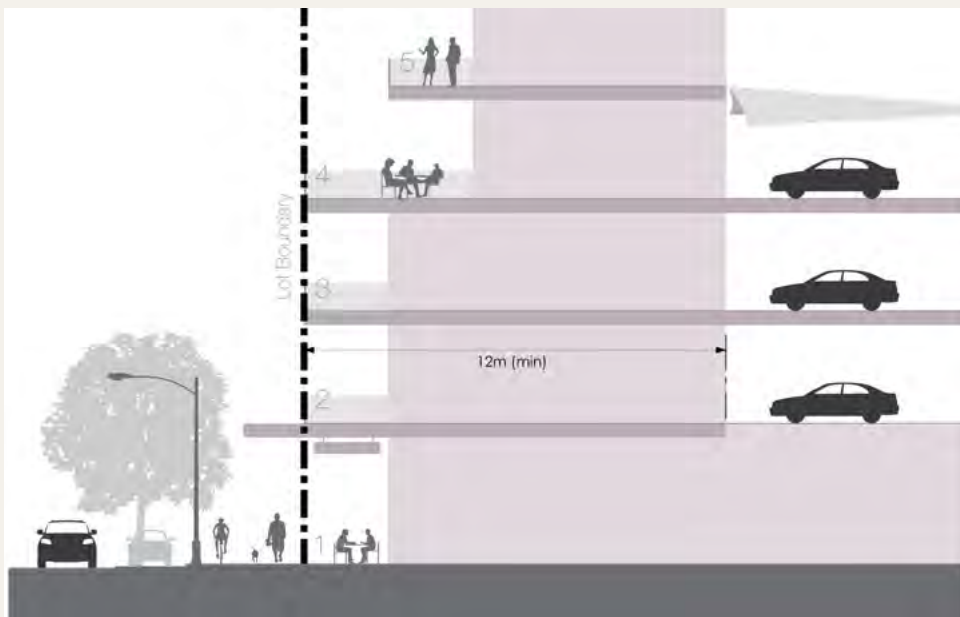


FIGURE 10: MIXED USE R-AC0 AND RESIDENTIAL 2 R-AC0 PARKING - ABOVE GROUND CAR PARKS SLEEVED BEHIND UPPER FLOOR BUILT FORM AND SHADED



FIGURE 12: RESIDENTIAL 1 R-AC0 BASEMENT PARKING - NIL SETBACKS TO STREET BOUNDARY (APPROPRIATE SCREENING SHALL BE PROVIDED TO CAR PARKS)

2.3.5 LANEWAYS

DESIGN INTENT

Laneways in an urban environment function primarily to provide vehicular access to parking areas, service and loading areas.

To provide a safe environment, development abutting laneways should provide an opportunity for passive surveillance of the laneways through design. Development abutting laneways should be designed with windows and openings from habitable spaces overlooking the laneway.

OBJECTIVE

To provide for an interesting and engaging environment allowing for passive surveillance from surrounding development and to ensure the laneway functions as a shared use vehicle and service access zone.

ACCEPTABLE DEVELOPMENT CRITERIA

- Embayed single aisle parking, separated by street trees is permitted whilst allowing two way movement and passing areas.
- Street trees are to be provided by the applicant.

Notes:

The City's Policy 'Development abutting rights of ways' may be applicable in the context of a development fronting onto an existing right of way/laneway.

Where there is a conflict this Local Development Plan and the above policy, the Local Development Plan shall prevail.

The north-south laneways between Grand Promenade and Herbert, and Huntriss Road and Sydenham Road (south of Scarborough Beach Road), are both category 4 laneways; and therefore access will not be provided by the City.



Examples of laneways incorporating streetscape improvements including landscaping and parallel parking

2.3.6 BICYCLE PARKING FACILITIES

DESIGN INTENT

To provide convenient end of trip facilities and secure bicycle parking, as part of developments in the precinct, to encourage the use of bicycles as a convenient form of transport.

OBJECTIVE

Developments will provide safe and secure bicycle storage and end-of trip facilities.

ACCEPTABLE DEVELOPMENT CRITERIA

- The incorporation of bicycle parking within a built form development is to be assessed against the City's Bicycle Parking policy (as amended).



Examples of bicycle parking to be provided on site

2.3.7 SERVICE AREAS AND FACILITIES

DESIGN INTENT

To ensure that the servicing aspects of a building do not have a negative effect upon the amenity of the building, surrounding development or the public realm.

OBJECTIVES

- Development shall provide dedicated and appropriate access and parking facilities for service vehicles to pick up goods from and deliver goods to the subject site.
- Ensure that the service areas and facilities are designed as an integral component of the building.

ACCEPTABLE DEVELOPMENT CRITERIA

- Development shall identify the access and parking arrangements for service delivery vehicles, including vehicle swept paths and dedicated parking/standing locations adjacent to storage areas.
- All service and delivery vehicles should be able to enter and leave sites in a forward gear.
- Air conditioning units, loading areas, service areas and outdoor clothes drying areas should be located and designed to minimise their visibility from, and impact on the amenity of the public realm.

2.3.8 PARKING PROVISIONS

DESIGN INTENT

The benefits of Activity Corridor Design can be optimised in the precinct, given the proximity to the proposed future Rapid Transit System along Scarborough Beach Road, the growth of mixed use nodes within close walking distance and the proposal for dedicated cycle paths. This enables the opportunity to set maximum car parking requirements at a level below that of the City of Stirling Parking Policy and Residential Design Codes in the future. This in turn will support the broader sustainability goals of the project whilst encouraging the use of alternative modes of transport and a reduction of private car use and contribute to the delivery of a more pedestrian friendly environment within the precinct.

OBJECTIVE

Provide safe parking for residents and workers whilst limiting the number of car bays provided and promoting the utilisation of alternative modes of transport.

ACCEPTABLE DEVELOPMENT CRITERIA

- Provision for residential parking (and residential parking with a mixed use development) shall be in accordance with R Codes, Section 6.3.3. Up to 50% of the required visitor parking bays may be provided on-street provided that they are located adjacent to the site.
- Provision for non-residential parking shall be provided at a flat rate of 3.5 spaces per 100 square metres of gross leasable area, except for a shop over 500 square metres which shall provide 5.0 spaces per 100 square metres of gross leasable area.

2.4.1 BUILDING LAYOUT AND ORIENTATION

DESIGN INTENT

The design of all new developments should take into account, solar access, with built form positioned to reduce overshadowing to neighbouring residential properties and to take advantage of natural cross ventilation and access to natural light.

OBJECTIVE

- To enhance the amenity of residents and workers, habitable rooms in all buildings are to be designed in a manner that optimises solar access and natural ventilation and minimises overshadowing of adjacent buildings.

ACCEPTABLE DEVELOPMENT CRITERIA

- A minimum of 70% of all dwellings must receive at least 2 hours direct sunlight to major openings and private open space between 9am and 3pm mid winter.
- All developments shall be designed and orientated to optimise natural cross ventilation through dwellings and energy conscious building design.
- Any developments over 3 storeys in height is to be accompanied by an overshadowing diagram demonstrating the potential impact of the buildings shadow on surrounding development sites.



Examples of buildings optimising solar access and natural ventilation

2.4.2 COMMUNAL OPEN SPACE

DESIGN INTENT

Communal open spaces within mixed use and residential developments provide for valuable congregation and recreational opportunities. Areas should be of a size and scale that is usable for both active and/or passive recreation. The spaces should be attractive and encourage social interaction incorporating seating, paved areas and planting.

OBJECTIVE

To create attractive places for residents and workers to recreate and socialise and to encourage the planting of on-site vegetation for an improved local environment.

ACCEPTABLE DEVELOPMENT CRITERIA

- All residential developments (or residential component of a mixed use development) of:
 - Between 6-12 units (inclusive), shall have a communal open space of not less than 40m² with a minimum dimension of 5m, accessible from a major opening to a habitable room, or
 - Between 13-20 units (inclusive), shall have a communal open space of not less than 80m² with a minimum dimension of 5m, accessible from a major opening to a habitable room; or
 - Over 20 units, shall have a communal open space of not less than 10% of the Gross Lot Area with a minimum dimension of 8m, accessible from a major opening to a habitable room.
- Communal open spaces are garden areas which exclude voids and undercroft areas and car parks.
- Communal space within mixed use developments shall include shade trees, lighting and seating.
- Open Space provision on site shall be in accordance with the Specific Guidelines (refer sections 3.1.1 - 3.1.4).



Well planted, attractive communal open space for residents and workers to recreate

2.4.3 LANDSCAPE

DESIGN INTENT

The landscape design will be of a scale and nature that relates to the ultimate scale of buildings and spaces. Landscape treatments contribute to the overall character and qualities of the site and its context. The landscape design should enhance the experience of users of the development and provide a valued aesthetic to the broader urban environment.

OBJECTIVE

To create urban landscapes that are sustainable and long lasting, and ensure that soft landscape treatments of developments and private land, complement the public realm, creating a rich 'green' environment.

ACCEPTABLE DEVELOPMENT CRITERIA

- A Landscape Plan shall be submitted as a component of a Development Application within the subject area.
- Tree species shall be specified wherever possible. Native and indigenous species shall be used and selected for the individual site conditions, solar access and amenity.
- All development shall integrate planting and soft landscape areas with drainage infrastructure as a means for passive irrigation.
- Where treatments about the public realm, paving, furniture and planting treatments shall be complimentary to the streetscape.

Note: To be in accordance with the City's 'Landscaping Policy'.



Urban landscape design enhancing the experience for residents and complementing the public realm

2.4.4 LIGHTING

DESIGN INTENT

Lighting of a building facade enhances legibility and safety.

OBJECTIVE

To provide a safe and secure environment at night, enhance the built form and public realm and create interest.

ACCEPTABLE DEVELOPMENT CRITERIA

- Lighting shall be incorporated under awnings and colonnade structures.
- Lighting shall enhance residential and commercial entries and communal open space for safe and convenient night time use.



Examples of lighting enhancing the built form and public realm

2.4.5 STORAGE AREAS & RUBBISH COLLECTION ENCLOSURES

DESIGN INTENT

Development shall be designed to cater for the storage of goods and rubbish on site in areas sheltered from the weather and outside of public viewing areas.

OBJECTIVE

Development shall provide dedicated and purpose built structures for the storage of goods, bins and rubbish on site.

ACCEPTABLE DEVELOPMENT CRITERIA

- Development Applications shall identify the location and structure of dedicated goods and rubbish storage areas directly accessible or adjacent to the pathway of rubbish removal vehicles.
- Dedicated goods and rubbish storage areas are to be located on-site and are not to be visible from the public realm.

Note: To be in accordance with the City's 'Bin Storage Areas Policy'.

2.5.1 ENTRY POINTS

DESIGN INTENT

Building entries provide an interface with the public domain and generate 'people presence', thereby contributing to the activation of the street. The entry point directs and orientates the visitor and can create a desirable identity for a development. The entry is also the front door for many residential dwellings and therein the residents 'sense' or 'place' of address in the street. Entries may lead into a common entry foyer or directly into communal open space from the street.

OBJECTIVE

To contribute to variation, orientation and articulation within the streetscape and to provide a clearly defined address at street level for all development.

ACCEPTABLE DEVELOPMENT CRITERIA

- The building entry shall be a clearly identifiable element within the facade design at all times of day.
- The primary entry to a building's upper floors shall be accessed and addressed from the primary or secondary street.
- Building design shall provide sheltered, well-lit and highly visible spaces to enter the building.
- Development shall provide clear sight lines between the street and the entry foyer space to provide for a safe environment.
- Development shall provide separate entry points for vehicles and pedestrians and for different uses within the building, for example between residential and commercial uses.
- All built form shall have a clearly identified street number.
- Lots adjacent to Scarborough Beach Road shall have their primary pedestrian entrance from Scarborough Beach Road.



Examples of entry points providing a clearly defined address at street level whilst contributing the streetscape activation

2.5.2 SAFETY

DESIGN INTENT

To produce an urban environment that is safe to use, the urban development should minimise the opportunity for anti-social behaviour and crime and maximise a sense of safety through the responsible design and management of urban spaces and buildings.

OBJECTIVE

The promotion of a secure community through the design of developments and spaces that provide safety and security for all users, residents, employees and visitors.

ACCEPTABLE DEVELOPMENT CRITERIA

- Built form development shall avoid the creation of entrapment areas, blind corners, dead ends, hidden recesses, narrow pathways and blank walls.
- Fencing to laneways is to comply with the City's *Development Abutting Rights of Way* policy.
- Loading bay access lanes and other areas that may be dead ends shall be secured.

2.5.3 FENCING & GATES

DESIGN INTENT

To provide secure separation and delineation of private and public spaces in a manner that respects the overall aesthetic quality of new development.

OBJECTIVE

Ensure fencing and gates are incorporated into the design of buildings and spaces in a manner that compliments and does not detract from the urban character.

ACCEPTABLE DEVELOPMENT CRITERIA

- Fences installed along the frontage of a property or any secondary street boundary shall not restrict passive surveillance opportunities of the street.
- Fencing to laneways shall be visually permeable above 1.2m high.
- Fences and gates shall be designed in a manner that positively contributes to the overall streetscape.

Note: The City's Streetscapes policy (as amended) may be applicable in the assessment of fencing and gates as a component of a Development Application.

2.5.4 SIGNAGE

DESIGN INTENT

Signage is important for direction and for business identification and should be carefully considered in the design of buildings. Signage should be compatible with the desired streetscape character. Where not appropriately considered, signage can appear as a later addition and have a considerable negative impact on the amenity of the building, surrounding developments and the public realm. Signage should therefore be designed as an integral component of the building design to improve the overall appearance of a building. Signage design should consider scale and proportion of the development and information hierarchy within the street context without obscuring or dominating important views.

OBJECTIVES

- To ensure signage is in keeping with the desired streetscape character and with the development in scale, detail and overall design.
- To ensure that signage is planned and integrated into the design and development process and avoids visual clutter.

ACCEPTABLE DEVELOPMENT CRITERIA

- The incorporation of signage within a built form development is to be assessed against the City's Advertising Signs policy (as amended).



Examples of signage integrated within building design and avoiding visual clutter

2.6.1 WATER EFFICIENCY

DESIGN INTENT

The landscape design will conserve, treat and recycle water where practicable.

OBJECTIVES

- Employ widespread use of water sensitive urban design (WSUD) techniques to reduce water usage and create a more sustainable environment.
- Reduce impact on surrounding ecosystem and human health from stormwater run-off.

ACCEPTABLE DEVELOPMENT CRITERIA

- The accommodation of all drainage shall be provided within development sites.
- Collect stormwater run-off in tree pits, drainage swales and rain gardens to passively irrigate plants.
- Use of native waterwise plant species which require less water.
- Use of mulch to reduce water loss due to evaporation.
- Landscape irrigation from grey (waste) water recycling techniques.
- Use of permeable paving to reduce stormwater run-off and to allow rain water to infiltrate the ground and replenish ground water.

2.7.1 CEDING OF LAND FOR PUBLIC PURPOSES

DESIGN INTENT

This Local Development Plan identifies the land required for infrastructure upgrades in accordance with the widening of Scarborough Beach Road (Refer Planning Control Area 123) and laneways under the City of Stirling Provisions of Local Planning Scheme No 3.

Land ceded for road reserve is to be provided free of cost.

OBJECTIVES

Developer cooperation is primarily required to facilitate:

- The creation of improved Scarborough Beach Road throughout currently constrained areas to facilitate public transport;
- The creation of improved laneways to allow rear access to built form and to allow additional development density and primary access to built form where applicable, and
- The creation of high quality public space in areas devoid or deficient of such areas.

ACCEPTABLE DEVELOPMENT CRITERIA

- Road widening is applicable to Scarborough Beach Road in accordance with Planning Control Area 123.
- Road widenings are applicable to laneways, in accordance with the City's Development Abutting Rights of Ways Policy.
- Development bonuses are provided for ceding of land for the widening of Scarborough Beach Road as per Planning Control Area 123.
- Land is to be ceded free of cost in accordance with the Metropolitan Region Scheme at either development or subdivision stage.
- Minor developments (e.g. additions) will not trigger ceding of land.
- Land ceded can still be included in plot ratio calculations for assessment purpose.

3. SPECIFIC PROVISIONS

The specific provisions outline the detailed development requirements for individual lots within Mixed Use R-AC0, Residential 1 R-AC0 and Residential 2 R-AC0

Developments shall be assessed under one of the following sections, as outlined in **Figure 1: Land Use Plan**, to be determined at development application stage:

- **Section 3.1.1** *Mixed Use R-AC0 / Residential 2 R-AC0*
- **Section 3.1.2** *Residential 1 R-AC0 - Rear Laneway Access*
- **Section 3.1.3** *Residential 1 R-AC0 - No Laneway Access*
- **Section 3.1.4** *Residential 1 R-AC0 - Side And Rear Laneway Access*

Note: Residential R30 and Residential R40 Lots are to be controlled by the Residential Design Codes (as amended).

3.1.1 MIXED USE R-AC0/RESIDENTIAL 2 R-AC0 AREAS (1) (8)

BUILDING CONTROL				
All development to be in accordance with R-AC0 Residential Design Code provisions, except where otherwise specified.				
BUILDING HEIGHT				
Minimum Building Height to Scarborough Beach Road		2 storeys	O P	
Maximum Building Height to Scarborough Beach Road (for lots equal to or less than 1000 m ²)		4 storeys (7)	O	
Maximum Building Height to Scarborough Beach Road (for lots greater than 1000 m ²)		5 storeys (7)	O	
Maximum Building Height to Laneway		3 storeys	P	
Maximum Height (7)			Maximum Height of Walls Built Up to Boundary	
Top of External Wall	Top of External Wall (Concealed Roof)	Top of Pitched Roof	Maximum Height	Average
17m	18m	21m	8m (2 storeys) 11m (3 storeys)	7m (2 storeys) 10m (3 storeys)
PLOT RATIO				
Maximum Plot Ratio (exclusive of non-residential floor area)				1.25 (7)
FLOOR HEIGHTS				
Minimum ground floor ceiling height				3.4m
Minimum upper floor ceiling height(s)				2.4m
OPEN SPACE (AREA TO BE PROVIDED ON SITE)				
Overall minimum open space (% of site)		10%	N	
Minimum Communal Open Space (for developments over 12 dwellings)		80m ² (5)	N	
Minimum Communal Open Space (for developments over 6-12 dwellings)		40m ² (5)	N	

Notes:

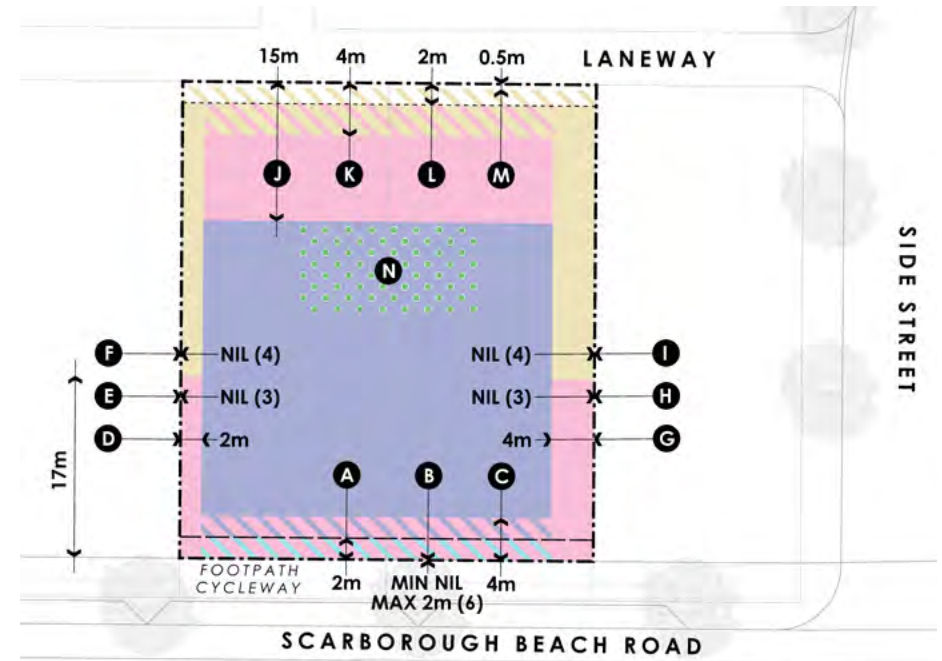
- (1) Subject to any additional measures in other elements of the R-Codes and DAP.
- (2) Setbacks from ultimate road widening.
- (3) Within 17m Scarborough Beach Road setback.
- (4) For up to two-thirds the length between 17m Scarborough Beach Road setback and 2m laneway setback.
- (5) Subject to section 2.4.2 Communal Open Space.
- (6) In character facade locations (refer section 2.2.3) ground floor character facade to be retained and second story building setback to Scarborough Beach Road shall be 2m.

BUILDING SETBACKS (MINIMUM DISTANCE FROM PROPERTY BOUNDARY)		
To Scarborough Beach Road (2)		
Ground Floor Building, 5th Storey Balcony/Roof Overhang	2m	A
2nd and 3rd Storey Building/Balcony, 4th Storey Balcony/No Roof Overhang	min. nil; max. 2m (6)	B
4th and 5th Storey Building	4m	C
To Northern or Western Side Boundary (9)		
4th and 5th Storey Building/Roof Overhang	2m	D
1st, 2nd and 3rd Storey Building	Nil (3)	E
1st and 2nd Storey Building	Nil (4)	F
To Southern or Eastern Side Boundary (10)		
4th and 5th Storey Building/Roof Overhang	4m	G
1st, 2nd and 3rd Storey Building	Nil (3)	H
1st and 2nd Storey Building/Balcony	Nil (4)	I
To Laneway Boundary (2)		
4th and 5th Storey Building, 5th Storey Balcony	15m	J
3rd Storey Building, 4th Storey Balcony	4m	K
1st and 2nd Storey Building, 3rd Storey Balcony	2m	L
2nd Storey Balcony	0.5m	M
To Side Street		
Setbacks as per Scarborough Beach Road (within 12m Scarborough Beach Road setback)		
Setbacks as per side boundary (outside of 12m Scarborough Beach Road setback)	-	

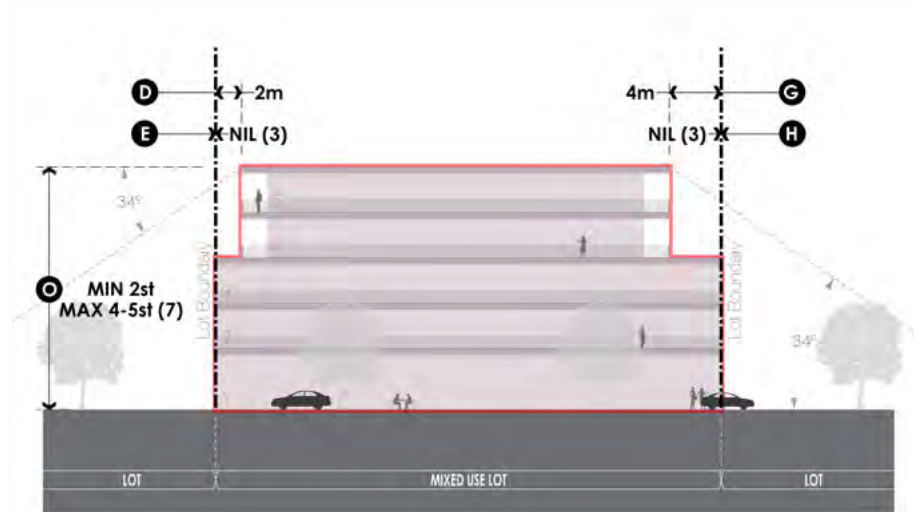
- (7) Additional plot ratio and height bonuses, subject to section 2.2.2.
- (8) Lot 592 Gildercliffe Street, Lots 631 & 637 Grand Promenade and Lot 757 Herbert Street (identified by * on Figure 1: Land Use Plan) are subject to Mixed Use R-AC0 'Permitted Land Uses' (refer section 2.1.1) and Residential 1 R-AC0 Areas – Side and Rear Laneway Access 'Building Type' Provisions (refer section 3.1.4).
- (9) All balconies to northern or western side boundary shall be setback a minimum 2m.
- (10) All balconies to southern or eastern side boundary shall be setback a minimum 4m.



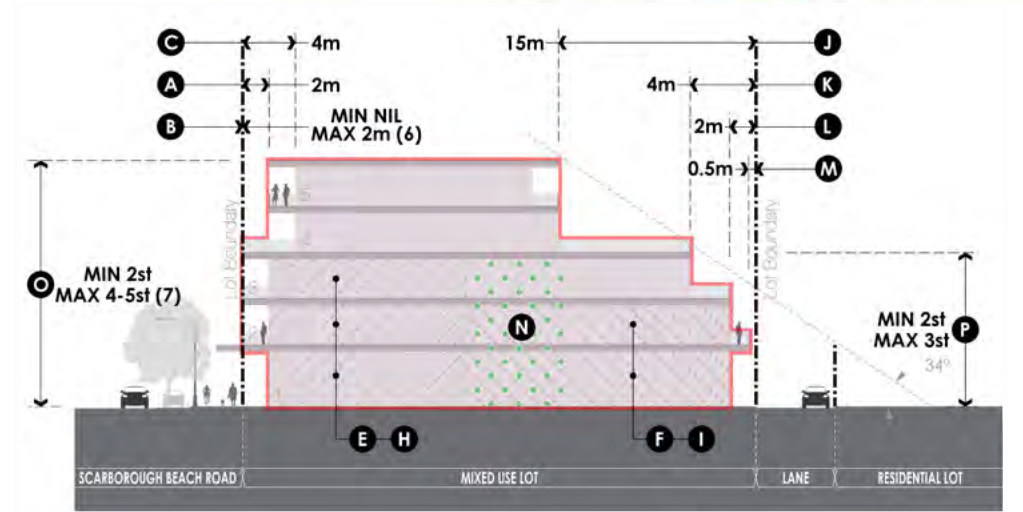
Indicative Artist Impression of Mixed Use R-AC0/Residential 2 R-AC0 Building Type



Plan illustrating Building Setbacks



Front View from Scarborough Beach Road - illustrating Building Setbacks and Envelope



Side View - illustrating Building Setbacks and Envelope

3.1.2 RESIDENTIAL 1 R-AC0 AREAS - REAR LANEWAY ACCESS (1)

BUILDING CONTROL				
All development to be in accordance with R-AC0 Residential Design Code provisions, except where otherwise specified.				
BUILDING HEIGHT				
Minimum Building Height to Scarborough Beach Road		2 storeys	K L	
Maximum Building Height to Scarborough Beach Road		3 storeys	K	
Maximum Building Height to Laneway		3 storeys	L	
Maximum Height (5)			Maximum Height of Walls Built Up to Boundary	
Top of External Wall	Top of External Wall (Concealed Roof)	Top of Pitched Roof	Maximum Height	Average
10m	11m	15m	7m	6m
PLOT RATIO				
Maximum Plot Ratio (exclusive of non-residential floor area)				1.0 (5)
OPEN SPACE (AREA TO BE PROVIDED ON SITE)				
Overall minimum open space (% of site)		10%	J	
Minimum Communal Open Space (for developments over 12 dwellings)		80m ² (4)	J	
Minimum Communal Open Space (for developments over 6-12 dwellings)		40m ² (4)	J	

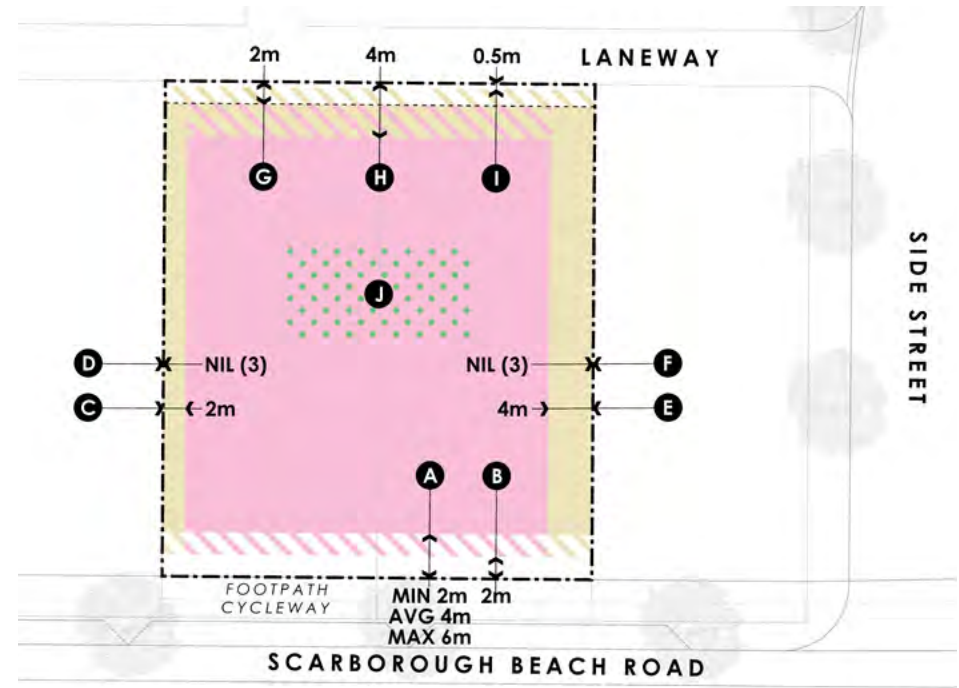
Notes:

- (1) Subject to any additional measures in other elements of the R-Codes and DAP.
- (2) Setbacks from ultimate road widening.
- (3) For up to two-thirds the length between the Scarborough Beach Road and laneway setbacks.
- (4) Subject to section 2.4.2 Communal Open Space.
- (5) Additional plot ratio and height bonuses, subject to section 2.2.2.
- (6) All balconies to northern or western side boundary shall be setback a minimum 2m.
- (7) All balconies to southern or eastern side boundary shall be setback a minimum 4m.
- (8) Includes bonus 4th storey where applicable under section 2.2.2.

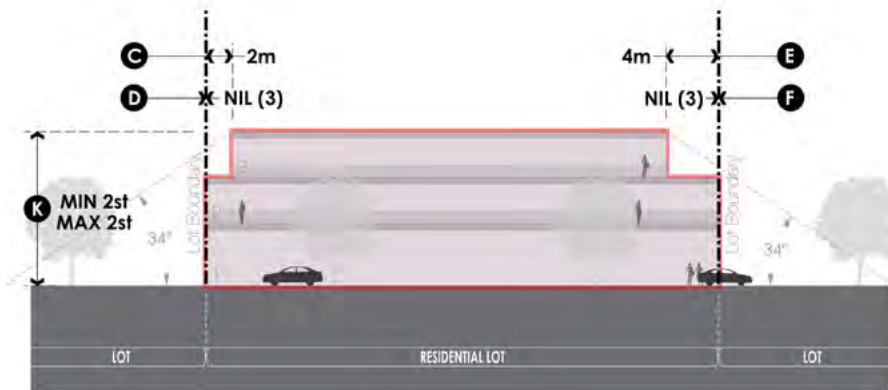
BUILDING SETBACKS (MINIMUM DISTANCE FROM PROPERTY BOUNDARY)		
To Scarborough Beach Road (2)		
1st, 2nd and 3rd Storey Building (8)	min 2m; avg 4m; max 6m	A
2nd and 3rd Storey Balcony/Roof Overhang (8)	2m	B
To Northern or Western Side Boundary (6)		
3rd Storey Building (8)	2m	C
1st and 2nd Storey Building	Nil (3)	D
To Southern or Eastern Side Boundary (7)		
3rd Storey Building (8)	4m	E
1st and 2nd Storey Building	Nil (3)	F
To Laneway Boundary (2)		
1st and 2nd Storey Building, 3rd Storey Balcony (8)	2m	G
3rd Storey Building (8)	4m	H
2nd Storey Balcony	0.5m	I
To Side Street		
Building/Balcony to all Storeys	2m	



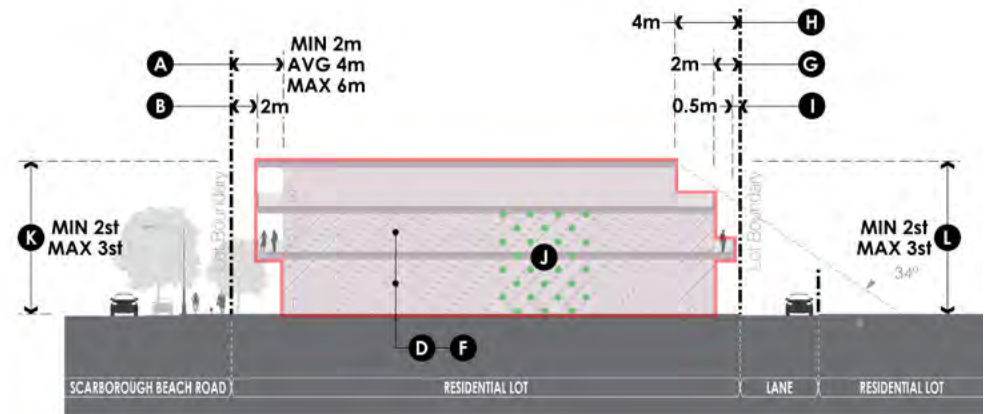
Indicative Artist Impression of Residential 1 R-AC0 Building Type (with Laneway Access)



Plan illustrating Building Setbacks



Front View from Scarborough Beach Road - illustrating Building Setbacks and Envelope



Side View - illustrating Building Setbacks and Envelope

3.1.3 RESIDENTIAL 1 R-AC0 AREAS - NO LANEWAY ACCESS (1)

BUILDING CONTROL				
All development to be in accordance with R-AC0 Residential Design Code provisions, except where otherwise specified.				
BUILDING HEIGHT				
Minimum Building Height to Scarborough Beach Road		2 storeys	I	
Maximum Building Height to Scarborough Beach Road and Laneway		3 storeys (6)	I	
Maximum Height (6)			Maximum Height of Walls Built Up to Boundary (12)	
Top of External Wall	Top of External Wall (Concealed Roof)	Top of Pitched Roof	Maximum Height	Average
10m (3 storeys)	11m (3 storeys)	15m (3 storeys)	7m	6m
13m (4 storeys) (9)	14m (4 storeys) (9)	18m (4 storeys) (9)		
PLOT RATIO				
Maximum Plot Ratio			1.0 (6)	
OPEN SPACE (AREA TO BE PROVIDED ON SITE)				
Overall minimum open space (% of site)		10%	H	
Minimum Communal Open Space (for developments over 12 dwellings)		80m ² (5)	H	
Minimum Communal Open Space (for developments over 6-12 dwellings)		40m ² (5)	H	

Notes:

- (1) Subject to any additional measures in other elements of the R-Codes and DAP.
- (2) Setbacks from ultimate road widening.
- (3) For up to two-thirds the length between Scarborough Beach Road and rear boundary setbacks.
- (4) Boundary setback to be in accordance with:
 - i) Table 5 of the Residential R-Codes where the lot boundary abuts a lot with an R-AC0 coding; or
 - ii) Table 2A and Table 2B of the Residential R-Codes where the lot boundary abuts a lot with an R-Coding of less than R80; or
 - iii) Section 6.1.4 of the Residential R-Codes clause C4.3, C4.4 and C4.5 (or otherwise in accordance with the Design Principles) where a wall is proposed to be constructed to a side lot boundary abutting a lot with an R-AC0 coding; or
 - iv) Section 5.1.3 of the Residential R-Codes clause C3.2 (or otherwise in accordance with the Design Principles) where a wall is proposed to be constructed to a side lot boundary abutting a lot with an R-Coding of less than R80.

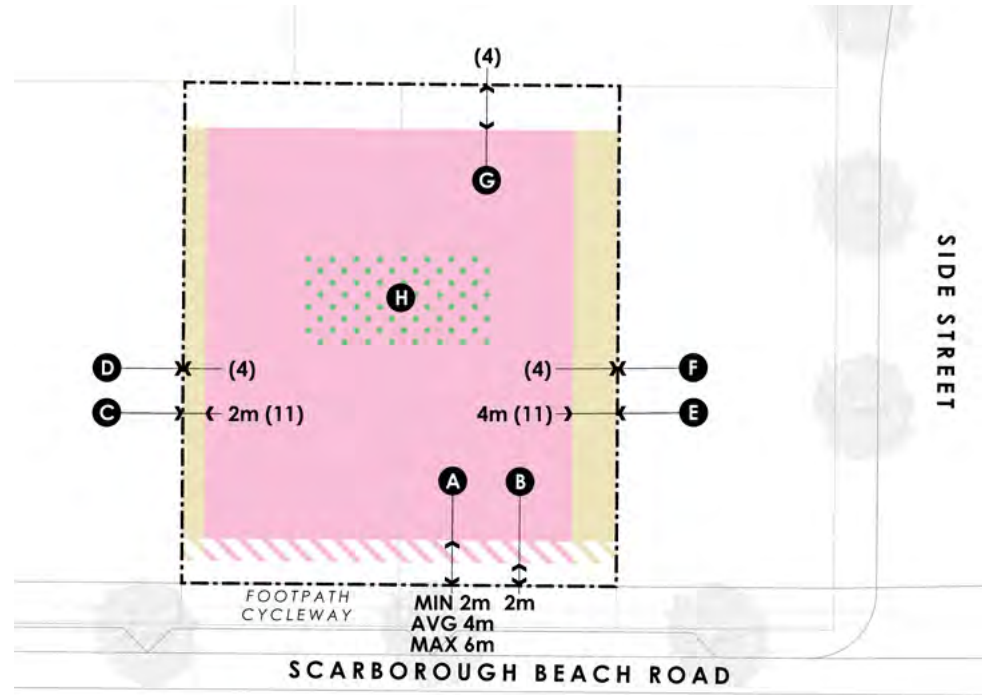
A nil setback to a rear boundary will not be permitted.

BUILDING SETBACKS (MINIMUM DISTANCE FROM PROPERTY BOUNDARY) (9)		
To Scarborough Beach Road (2)		
1st, 2nd and 3rd Storey Building (10)	min 2m; avg 4m; max 6m	A
2nd and 3rd Storey Balcony/Roof Overhang (10)	2m	B
To Northern or Western Side Boundary (7)		
3rd Storey Building (10)	2m (11)	C
1st and 2nd Storey Building	(4)	D
To Southern or Eastern Side Boundary (8)		
3rd Storey Building (10)	4m (11)	E
1st and 2nd Storey Building	(4)	F
To Rear Boundary		
1st, 2nd and 3rd Storey Building/Balcony (10)	(4)	G
To Side Street		
1st, 2nd and 3rd Storey Building/Balcony (10)	2m	

- (5) Subject to section 2.4.2 Communal Open Space.
- (6) Additional plot ratio and height bonuses, subject to section 2.2.2.
- (7) All balconies to northern or western side boundary shall be setback a minimum 2m.
- (8) All balconies to southern or eastern side boundary shall be setback a minimum 4m.
- (9) Subject to achieving 1 storey height bonus (refer section 2.2.2), 4th storey rear setback to be 10m; all other building setbacks to 4th storey building/balcony to be as per 3rd storey.
- (10) Includes bonus 4th storey where applicable under section 2.2.2.
- (11) Unless lot boundary abuts a lot with a R-Coding of less than R80, whereby setback is to be in accordance with Table 2A and Table 2B of the Residential R-Codes.
- (12) Walls built up to two side lot boundaries may be permissible subject to compliance with Table 4 of the Residential Design Codes and a maximum length of two-thirds the length of the boundary (or otherwise in accordance with the Design Principles of clause 6.1.4 of the Residential Design Codes).

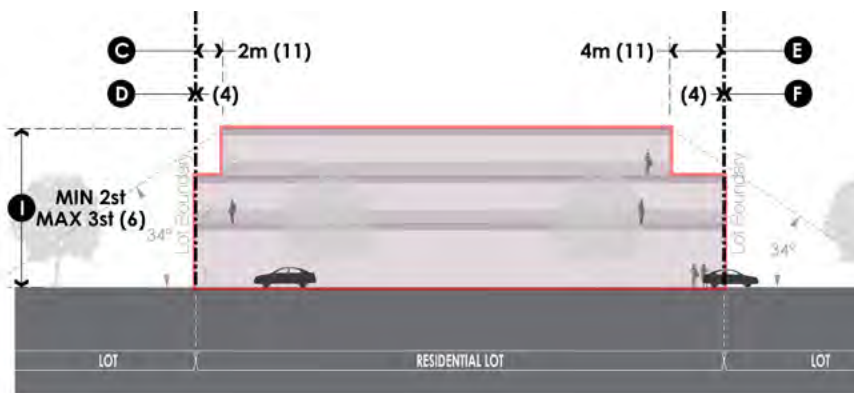


Indicative Artist Impression of Residential 1 R-AC0 Building Type (without Laneway Access)

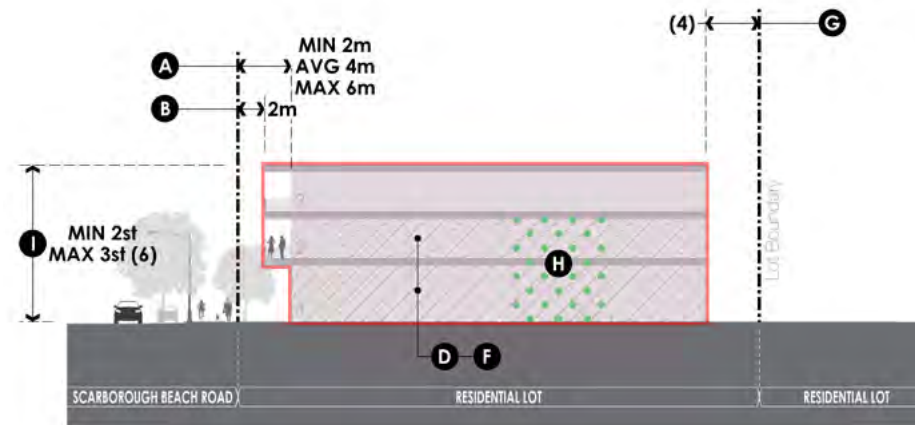


Plan illustrating Building Setbacks

- | | | | |
|-------------------|---------------------------|--------------------------|---------------------------------------|
| PROPERTY BOUNDARY | MAXIMUM 3 STOREY BUILDING | 2ND & 3RD STOREY BALCONY | PREFERRED CENTRAL OPEN SPACE LOCATION |
| | MAXIMUM 2 STOREY BUILDING | 2ND STOREY BALCONY | BUILDING ENVELOPE |



Front View from Scarborough Beach Road - illustrating Building Setbacks and Envelope



Side View - illustrating Building Setbacks and Envelope

3.1.4 RESIDENTIAL 1 R-AC0 AREAS - SIDE AND REAR LANEWAY ACCESS (1)

BUILDING CONTROL				
All development to be in accordance with R-AC0 Residential Design Code provisions, except where otherwise specified.				
BUILDING HEIGHT				
Minimum Building Height to Side Street		2 storeys	K	
Maximum Building Height to Side Street and Laneway(s)		3 storeys	K	
Maximum Height			Maximum Height of Walls Built Up to Boundary	
Top of External Wall	Top of External Wall (Concealed Roof)	Top of Pitched Roof	Maximum Height	Average
10m	11m	15m	7m	6m
PLOT RATIO				
Maximum Plot Ratio				1.0
Open Space (Area to be Provided On Site)				
Overall minimum open space (% of site)		10%	J	
Minimum Communal Open Space (for developments over 12 dwellings)		80m ² (5)	J	
Minimum Communal Open Space (for developments over 6-12 dwellings)		40m ² (5)	J	

Notes:

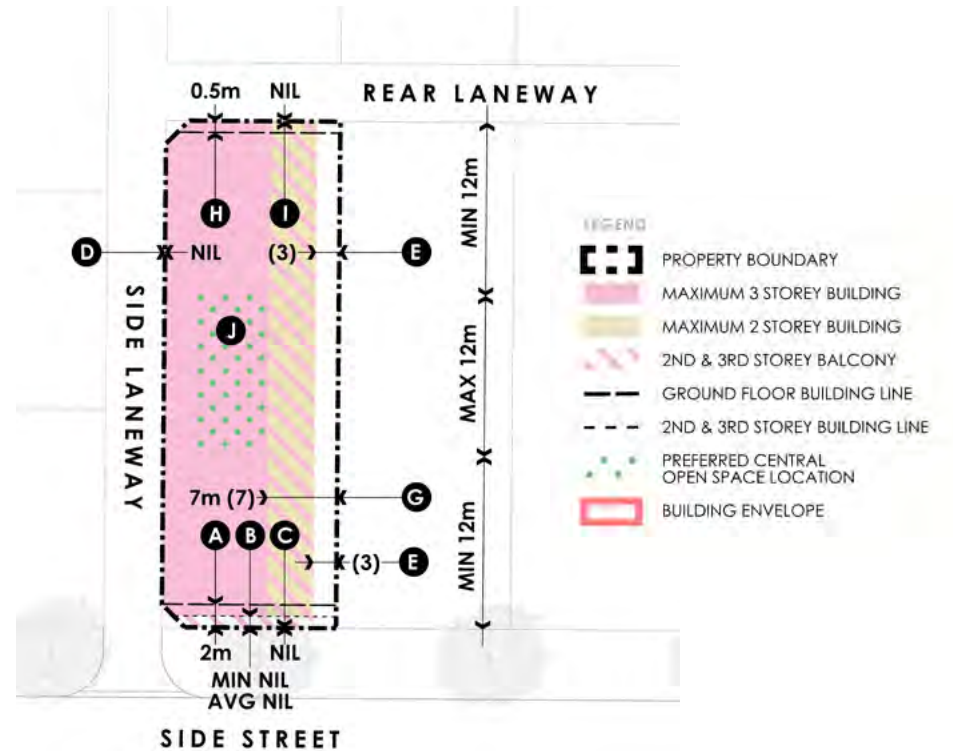
- (1) Subject to any additional measures in other elements of the R-Codes and DAP.
- (2) Setbacks from ultimate road widening.
- (3) Boundary setback to be in accordance with:
 - i) Table 5 of the Residential R-Codes where the lot boundary abuts a lot with an R-AC0 coding; or
 - ii) Table 2A and Table 2B of the Residential R-Codes where the lot boundary abuts a lot with an R-Coding of less than R80; or
 - iii) Section 6.1.4 of the Residential R-Codes clause C4.3, C4.4 and C4.5 (or otherwise in accordance with the Design Principles) where a wall is proposed to be constructed to a side lot boundary abutting a lot with an R-AC0 coding; or
 - iv) Section 5.1.3 of the Residential R-Codes clause C3.2 (or otherwise in accordance with the Design Principles) where a wall is proposed to be constructed to a side lot boundary abutting a lot with an R-Coding of less than R80.

Building Setbacks (minimum distance from property boundary)		
To Side Street		
Ground Floor Building	2m	A
2nd and 3rd Storey Building	min nil avg 1m	B
2nd and 3rd Storey Balcony/Roof Overhang	Nil	C
To Side Laneway Boundary (2)		
1st, 2nd and 3rd Storey Building/Balcony	Nil	D
To Side Lot Boundary		
1st and 2nd Storey Building/Balcony 3rd Storey Balcony/No Roof Overhang	(3)	E
3rd Storey Building/Roof Overhang	7m (7)	G
To Rear Laneway Boundary (2)		
Ground Floor Building	0.5m	H
2nd and 3rd Storey Building/Balcony	Nil	I

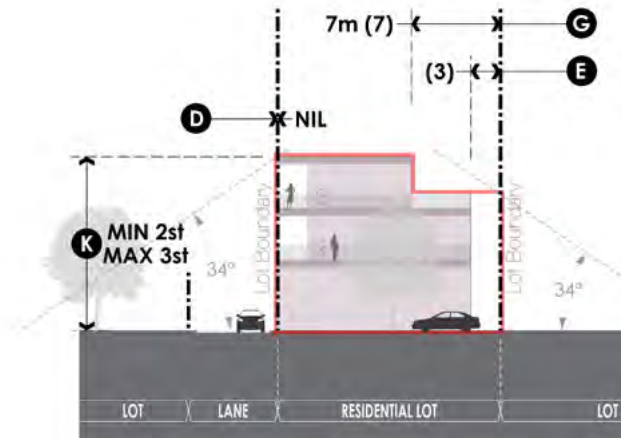
- (4) Up to 12m in length between 12m side street setback and 12m laneway setback.
- (5) Subject to section 2.4.2 Communal Open Space.
- (6) All balconies shall be setback a minimum 2m.
- (7) Unless lot boundary abuts a lot with a R-Coding of less than R80, whereby setback is to be in accordance with Table 2A and Table 2B of the Residential R-Codes.



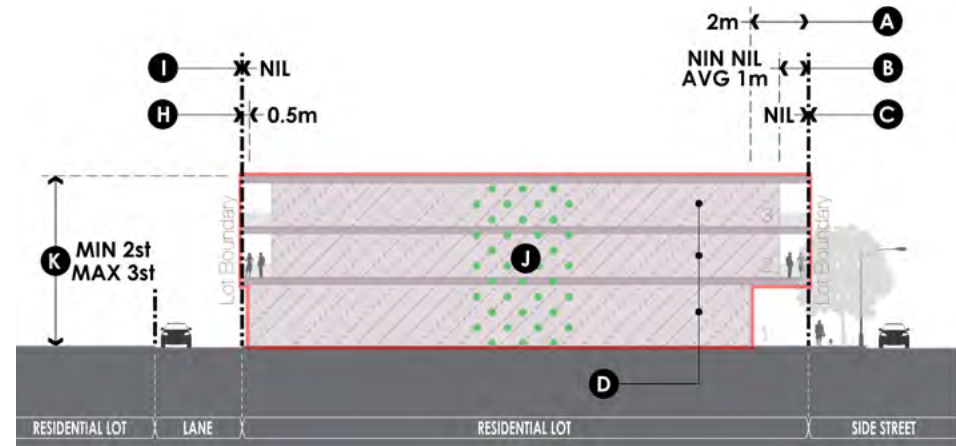
Indicative Artist Impression of Residential 1 R-AC0 Building Type (with side/rear Laneway Access)



Plan illustrating Building Setbacks



Front View from Side Street - illustrating Building Setbacks and Envelope



Side View from Side Laneway - illustrating Building Setbacks and Envelope

3.2.1 MUNRO RESERVE REDEVELOPMENT

The redevelopment of 'Munro Reserve' is the North-West portion of the Reserve at the intersection of Hancock Street and the adjacent laneway (refer **Figures 13-15**).

General Provisions of the contemplated redevelopment of the Mixed Use R-AC0 Site within Munro Reserve shall comply with the Design Intent outlined in Section 3.4.4 of the Scarborough Beach Road Strategy.

Should it occur, redevelopment shall be in accordance with the Specific Provisions of 'Mixed Use R-AC0/Residential 2 R-AC0' Lots (refer section 3.1.1), however the additional provisions shall apply:

Active Frontages

- The Southern and Eastern boundaries of the development site shall be treated as if they front Scarborough Beach Road frontages, in accordance with section 2.2.5.

Permitted Land Uses

- The Southern and Eastern boundaries of the development site shall be treated as if they front Scarborough Beach Road, in accordance with section 2.1.1.
- An active land use, such as a cafe, shall be provided adjacent to Munro Reserve (refer **Figures 16-17**).

Tree Retention

- Trees to be retained within Munro Reserve where practically possible.



 POTENTIAL DEVELOPMENT SITE

FIGURE 13: AERIAL VIEW OF SITE

Figures 23-26 illustrate indicative development only, subject to feasibility by the City.



FIGURE 14: INDICATIVE DEVELOPMENT (PLAN) IN MUNRO RESERVE

- LEGEND
- CONTEMPLATED DEVELOPMENT SITE
 - ACTIVE FRONTAGE REQUIRED (REFER SECTION 2.2.5)
 - ACTIVE LAND USE (I.E. CAFE)



FIGURE 15: INDICATIVE DEVELOPMENT (PERSPECTIVE) IN MUNRO RESERVE



FIGURE 16: ARTIST IMPRESSION OF A DAY CARE DEVELOPMENT IN MUNRO RESERVE

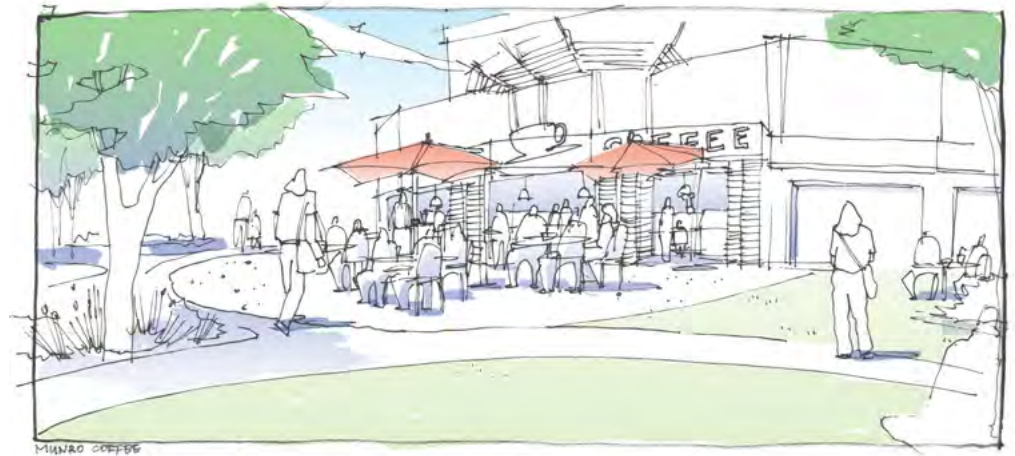


FIGURE 17: ARTIST IMPRESSION OF AN ACTIVE USE (I.E. CAFE) ACTIVATING THE ADJACENT MUNRO RESERVE

Scarborough Beach Road West Area

LOCAL DEVELOPMENT PLAN

